

Equipment Details

Bodies:	Aluminium extrusions welded and huck-bolted (FICAS Technology). Through gangways provided between car with internal and external bellow and overlapping sliding plates.
Bogies:	Bombardier Series 3 EMU 'H-frame'.
Couplers:	Dellner I 2 auto-couplers only on the front of DMOS cars, intermediate couplers between cars.
Traction System:	Bombardier 3 phase AC, three motored axles per motor car. Regenerative and rheostatic braking capability.
Compressor:	Knorr-Bremse VVI 20T oil free reciprocating – 3 phase motor.
Brakes:	Knorr-Bremse EP2002 with 2 actuators per axle
Auxiliary Power:	Bombardier Auxiliary Converter, two per train fitted to DMOS cars.
Supplies:	11 0V dc control system with 1 08V, 220Ah battery fitted on DMOS cars.
Saloon Lighting:	22 (1 6 saloon, 6 cab) Fluorescent T5 tubes via individual inverters per car.
Emergency Lighting:	17 (1 4 saloon, 2 cab, 1 connecting gangway) battery-fed fluorescent T5 tubes via individual inverters per car normally forming part of the main saloon lighting.
HVAC:	Two roof mounted saloon air conditioning modules per car supplying ceiling mounted air ducts. Separate module on DMOS cars for cab air conditioning.
Passenger Information:	An LED external facing front destination display and one external platform facing LED display per vehicle side. One double sided and two single sided body-end internal LED displays per car.
CCTV:	DOO CCTV displayed on two monitors in cab Saloon CCTV system viewable in cab and recorded digitally.
Doors:	Four electrically operated sliding doors per side, sliding pocket doors and configured as two double doorways per side. Fitted with obstacle detection plus threshold lighting when doors are open.



Class 378 Stock

London Overground



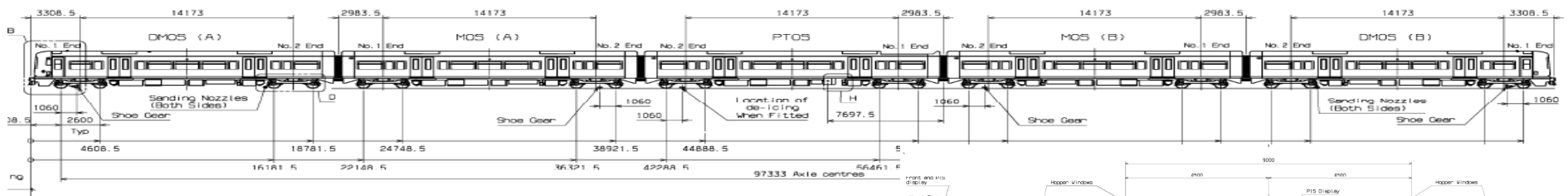
Built by Bombardier Transportation UK, Derby 2008 - 2015

Entered service in 2009

Maintained by: Bombardier Transportation

Principal Characteristics

Track Gauge:	1 435mm
Current system:	378/1 - 750V dc 3 rd rail only 378/2 - 750V dc 3 rd rail or 25kV ac OLHE
Types of vehicle:	DMOS : Driving Motor Open Saloon MOS : Motor Open Saloon PTOS : Pantograph Trailer Open Saloon
Formation per Unit:	5 car blocked train
Formation per unit (5):	DMOSA – MOS A – PTOS – MOSB – DMOSB
Number of Trains:	57 five cars
Operation:	Driver Only Operated (DOO)



Vehicle details and statistics

	DMOS	MOS	PTOS
Length over body end:	19995mm	19660mm	19660mm
Width of Body:	2800mm	2800mm	2800mm
Car Height:	3774mm	3774mm	3774mm
Tare weight:	45.5 tonnes	41.1 tonnes	39.9 tonnes
Tare weight of 5-car train:	212.8 tonnes inc gangways		
Passenger door open width:	1500mm	1500mm	1500mm
Car number series:	38001-38057 38101-38157	38201-38257 38401-38457	38301-38357
Vehicles in stock:	114	114	57
Grand total in stock:	285		

Passenger accommodation:

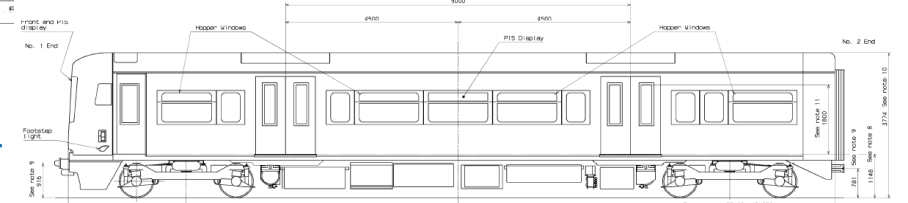
Please note that standing capacity figures exclude seating capacity

Seating capacity: (Number of full seats per train)	186
Seating capacity: (Number of tip up and perch seats, excluding wheelchair spaces)	84
Wheelchair spaces/additional tip up seats	2
Maximum average load standing capacity (seated plus 25% crush)	420
Maximum full load standing capacity (seated plus 70% crush)	840
Theoretical crush standing capacity (seated not including perch seats) plus 7 standing customers per m ² in vestibule areas and 6 in all other)	906

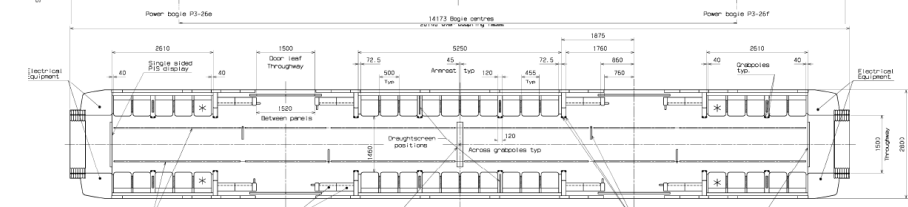
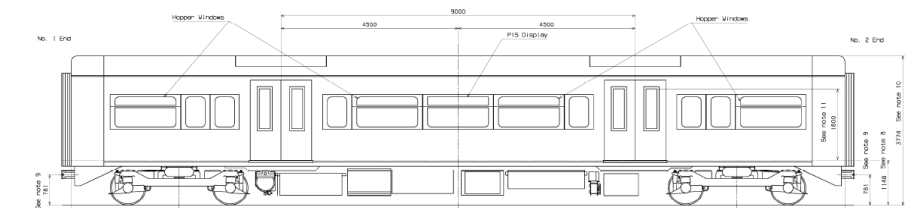
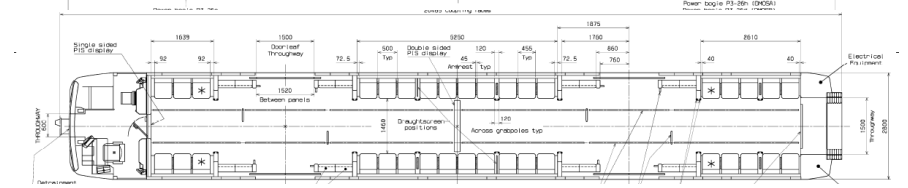
NOTES:

- Capacities here are figures calculated from floor area for design purposes
- For propulsion performance rating, tip up seats in use
- For structural and braking capacity (and JTC), tip up seats in use

DMOS Car



MOS car



PTOS car

