

METHOD STATEMENT

Feltham Major Works

29th May 2018 – March 2019

Document Control

ISSUE	DATE	STATUS	BY	CHK	APD
1.0	23/05/18	Draft method statement	KOS	DPH	DPH



Scheme Summary

As part of Feltham Town Centre improvement works, Hounslow Road will be temporarily closed to all traffic and pedestrians between New Road and Hanworth Road from Tuesday 29th May for 10 months (see Figure 1). This is primarily because Network Rail are extending the length of the existing platforms at Feltham Station to make way for longer ten-carriage trains, increasing capacity between Reading and London Waterloo. In order to do so, the existing level crossing on Bedford Lane will be permanently closed and replaced by a new pedestrian/cycle bridge over the railway line. However the level crossing will remain open for pedestrians, cyclists and southbound traffic until the bridge is constructed and Hounslow Road has reopened. Northbound traffic on Bedford Lane will be diverted via New Road/Hounslow Road.

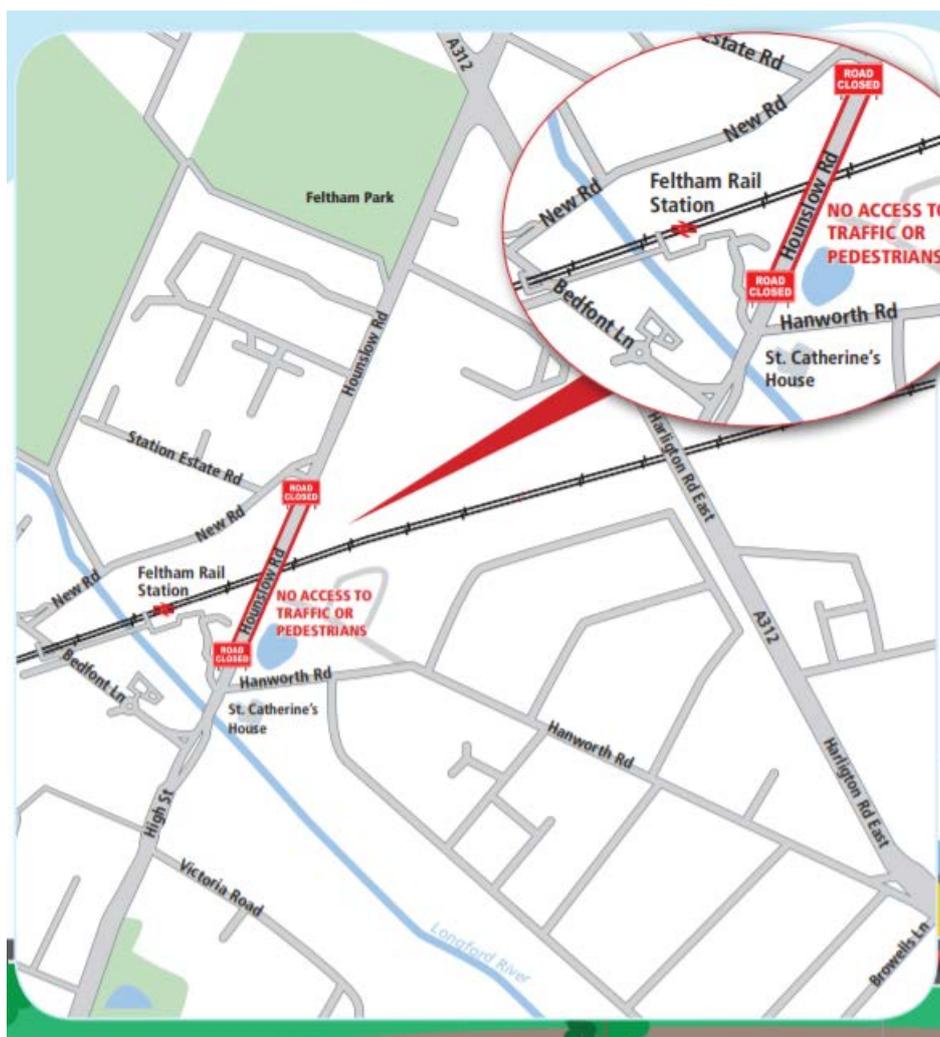


Figure. 1 Map showing closure point on Hounslow Road to all traffic and pedestrians

This method statement will focus on Phase 1 of these works, which will be in place from Tuesday, 29 May until 6th September 2018, and outlines:

1. Traffic Management to be put in place
2. Signed and likely Diversion Routes – Buses and General Traffic
3. Anticipated Impacts
4. Mitigations Strategies – Signal Timings
5. Additional Mitigation Strategies
6. Monitoring Strategy/Rota



1. Traffic Management – Phase I: Tuesday 29th May until Thursday 6th September 2018

Phase I of the works involves the use of two sets of temporary traffic signals:

- New Road/Hounslow Road (existing priority-controlled junction): Will operate using 3 stages (including a pedestrian stage) as shown in Figure 2.
- Hounslow Road/Hanworth Road/Station Approach/Bedfont Lane/High Street (existing signalised junction - (25/133): Will operate using 5 stages (including a pedestrian stage) as shown Figure 3.

The full version of the TM plan is also attached for reference.

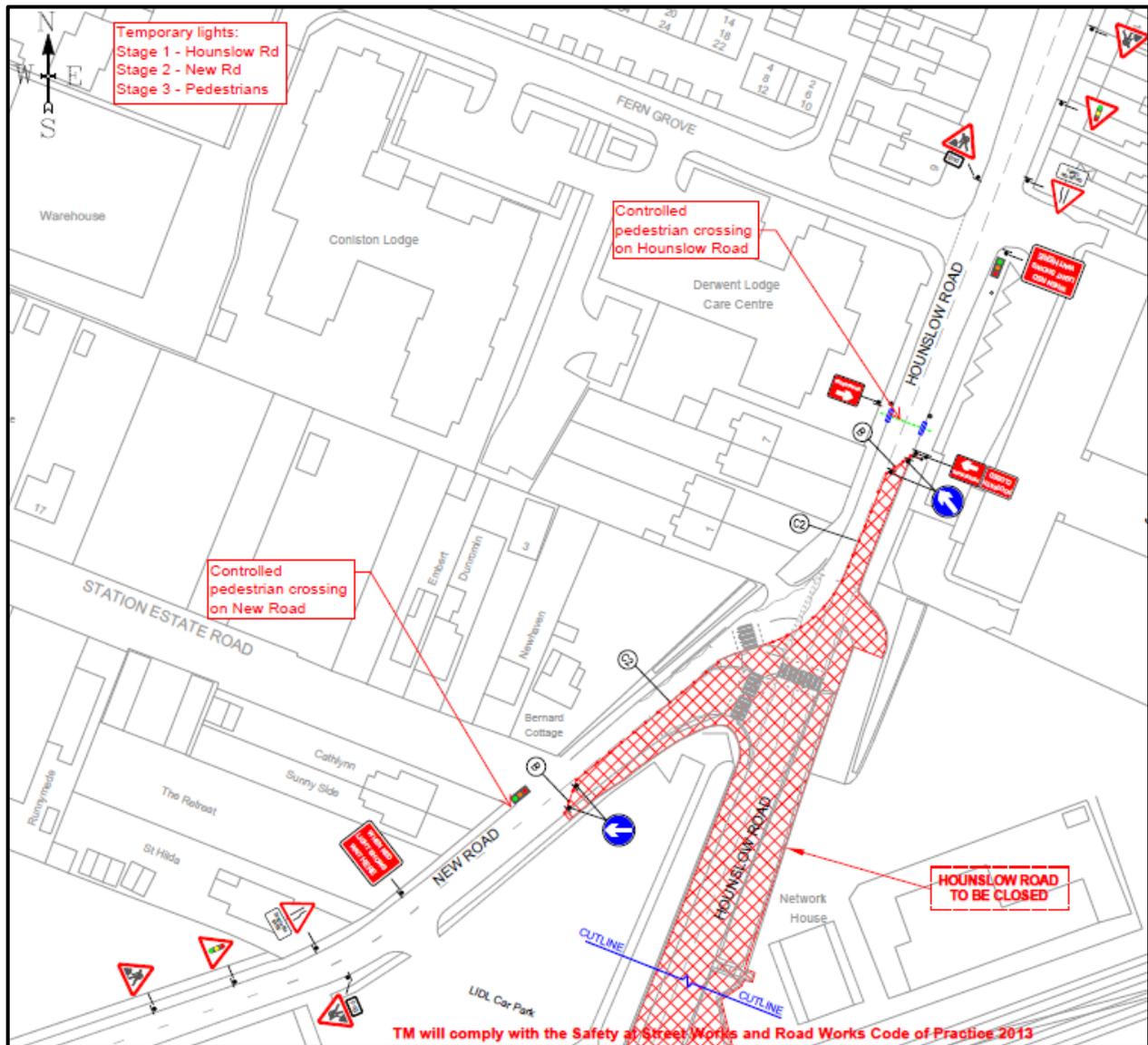


Figure 2. Traffic Management and Method of Control for temporary lights at existing priority junction of New Road/Hounslow Road.



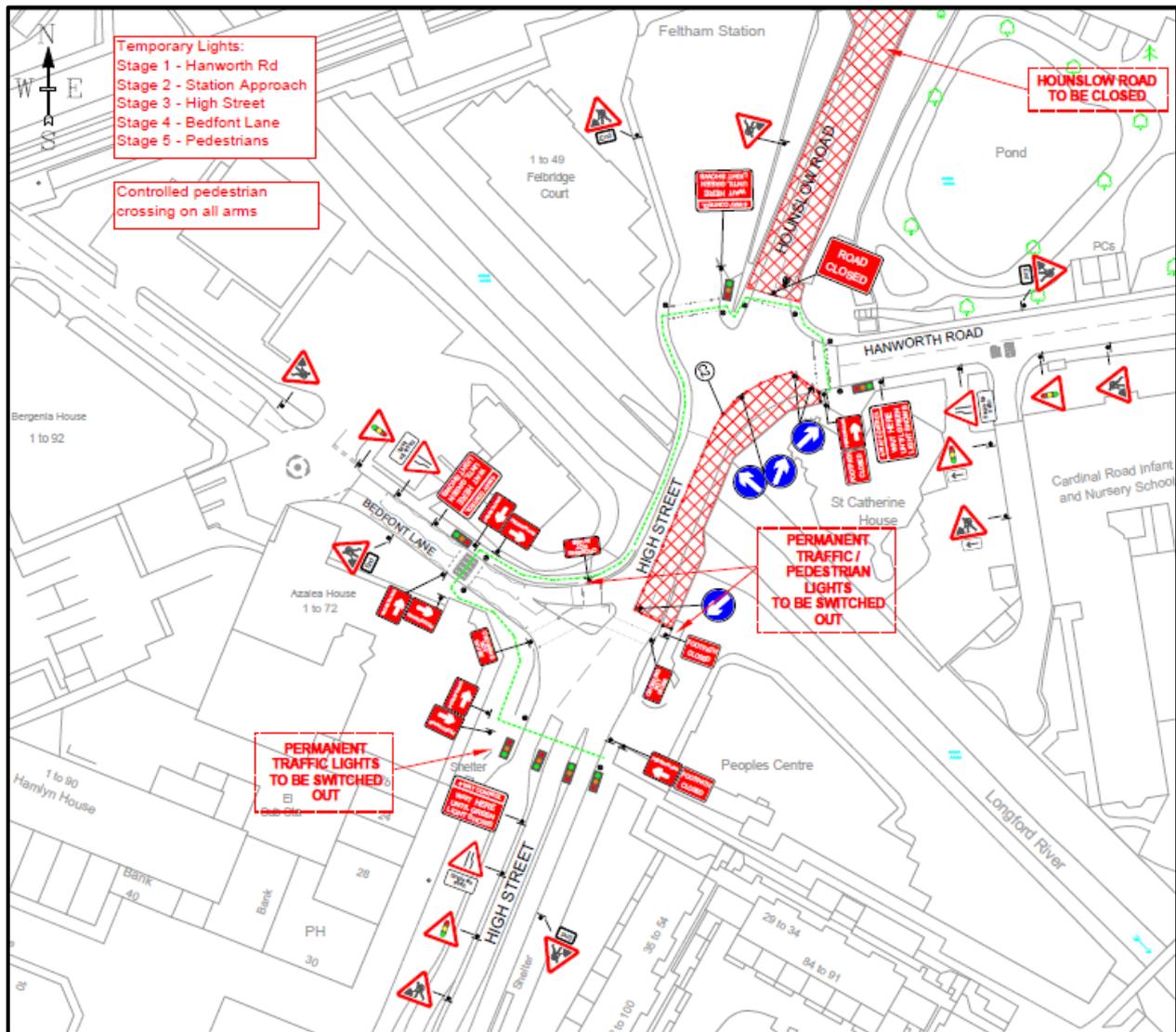


Figure 3. Traffic Management and Method of Control for temporary lights at the junction of Hounslow Road/Hanworth Road/Station Approach/Bedfont Lane/High Street

2. Diversion Routes

- **Eastbound on Bedfont Lane** – northbound traffic diverted via New Road/Hounslow Road.
- **Northbound on High Street Feltham (from Town Centre)** – diverted ahead via High Street, right along Hanworth Road, left back onto Harlington Road East
- **Southbound towards Feltham Town Centre** – diverted via Harlington Road East/Uxbridge Road, right onto Browells Lane and left back onto High Street Feltham.

These are the signed diversion routes for buses and general traffic, as publicised by Hounslow Council.



3. Anticipated Impacts

Due to the nature of the closure, and the volume of traffic which will be displaced it is expected there will be capacity issues at several junctions along this part of the network, and the impacts are likely to extend to the surrounding areas:

- **A312 Section – J/W Hounslow Road to J/W Browells Lane (R1130 & R1131):**
On UTC SCOOT control – capacity issues are expected at several junctions along this stretch, but contingency plans have been created and tested to assist bus and general traffic at both ends of the region. These will be adjusted as part of our monitoring activities during the first few days of the scheme going live to form our refined long-term strategy.
- **Junction of Browells Lane/Feltham High Street (25/257):**
Another key junction for bus diversion movements and currently on VA (local control). The green time for Browells Lane has been increased on VA in expectation of significantly increased flow entering and exiting Browells Lane.
- **Junction of Bedfont Lane/Hounslow Road (25/133) & New Road/Hounslow Road Junction:**
Will be on temporary signals for the duration of the works (Figure. 3). Several bus routes will divert through this junction, and so it is imperative that the temporary timings prioritise these routes. No site contact has yet been provided but should we feel the timings are inadequate we will provide alternative timings to Hounslow via email. This is also true of the new temporary signals at junction of New Road/Hounslow Road.
- It is likely the impacts will extend to the surrounding areas of the TLRN – A312, Staines Road and Feltham Town Centre. Additionally it may be necessary to hold traffic going into the area (see Mitigation Strategies) which may increase the level of queuing in adjacent regions.

4. Mitigation Strategies – Signal Timings

Mitigation methods will be focussed on assisting bus movements – however the published diversion route for buses is the same as that for general traffic so this should serve a dual purpose. All contingency procedures and plans are to be adapted to flow conditions during the first days of the scheme going live, based on observations of Network Managers monitoring the works and will be refined/confirmed before handing over to LSTCC:

- **25/29 (R1131):**



Relatively equal number of bus routes on S1 and S3.

New Method of Control introduced to allow buses to turn right from Hounslow Road north-east bound (S3).

Contingency procedures available:

3 levels of assist – increases the cycle time and splits additional time between S1 & S3:

C25-29_ASS..... Assists S1 and S3 at 25/29

C25-29_ASS_HEAVY..... Heavy Ass S1 and S3 at 25/29

C25-29_ASS_SEV..... Severe Ass S1 and S3 at 25/29

If observed that buses are not able to clear while turning right from Hounslow Road north-east bound (S3) – the procedure below introduces S4 - a NEB only stage:

C25-29_ASS_S4..... 25/29 - S4

- 25/47 (R1130):



All stages have bus diversion movements, but there will be a greater number of bus movements and significantly greater flow on Stage 2 (clearance stage/right-turn into Browells Lane) and Stage 3 (Browells Lane and Hanworth Road exits).

Contingency procedures available:

2 levels of assist which increase the time given to S3 – the lower cycle times are likely to be more efficient as S3 runs into a red light due to the internal stopline.

C25-47_S3_ASS_80S..... 80s - Ass S3 Browells Ln and Hanworth Rd at 25/47

C25-47_S3_ASS..... 88s - Ass S3 Browells Ln and Hanworth Rd at 25/47

C25-47_S3_ASS_HEV..... 96s - Ass S3 Browells Ln & Hanworth Rd at 25/47

Stage 2 does not run in the current method of control – there is a phase delay of 8s which is usually sufficient to clear the internal stopline of vehicles waiting to turn right into Browells Lane. Therefore if the right turn into Browells Lane does not clear, the procedures below introduce S2 which can then be increased as necessary:

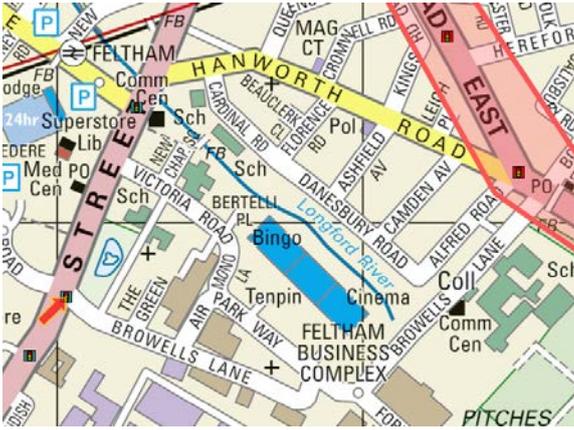
C25-47_S2_S3_ASS80..... 80s - Ass S2 (RT from A312) & S3 (Side Roads)

C25-47_S2_S3_ASS..... 88s - Ass S2 (RT from A312) & S3 (Side Roads)

C25-47_S2_S3_ASS_H..... 96s - Ass S2 (RT from A312) & S3 (Side Roads)



- 25/257 (VA):



Bus diversion movements on S1 and S3. VA max sets have been increased. These can be reviewed again as necessary.

- GATING:

It may be necessary to hold traffic northbound and southbound into the area in order to alleviate pressure on the network.

R630:

CR630_SB_HOLD..... Holds traffic SB into Feltham

R1116:

J25/028 P25 - A312 NB HOLD_SB ASSIST (STAGE 3) – USE ON THE SUBGROUP

All procedures and plans have been created for these works and so any feedback on their use or impacts would be greatly appreciated.

5. Additional Mitigation Strategies

Variable Message Signs

VMS have been set in numerous locations surrounding the area of the closure. The VMS have been set to advise road users ahead of the closure that extra disruption is likely, with the aim of encouraging traffic to use other routes.



6. NPD Monitoring in LSTCC

A rota has been devised providing full cover in LSTCC covering the first 3 days from 07:00 to 19:00. There will be both in station cover sitting in LSTCC and a representative on site to assist in areas where CCTV coverage is lacking. There is reasonable CCTV camera coverage on the A312, and some borough cameras in Feltham Town Centre itself.

After this point the situation will be reassessed, with the aim of the handing out of hours monitoring of the works back to LSTCC on Friday 1st June, with a full suite of procedures available to assist movements where necessary.

		Instation	Onsite
Mon 28th May	<i>Bank Holiday – works start</i>	N/A	N/A
	<i>Tuesday 29th May</i>		
Tue 29th May	AM into OP (07:00 - 12:00)	Claire Farrow	Katy O’Sullivan
	OP into PM (13:00 - 19:00)	Gareth Bek	Adam Greenland
Wed 30th May	AM into OP (07:00 - 12:00)	Gareth Bek	Adam Greenland
	OP into PM (13:00 - 19:00)	Daniel Hornshaw	Katy O’Sullivan
Thur 31st May	AM into OP (07:00 - 12:00)	Gareth Bek	Katy O’Sullivan
	OP into PM (13:00 - 19:00)	Naomi Steadman	Adam Greenland
Fri 1st June	AM into OP (07:00 - 12:00)	Katy O’Sullivan	N/A
	OP into PM (13:00 - 19:00)	Katy O’Sullivan	N/A

