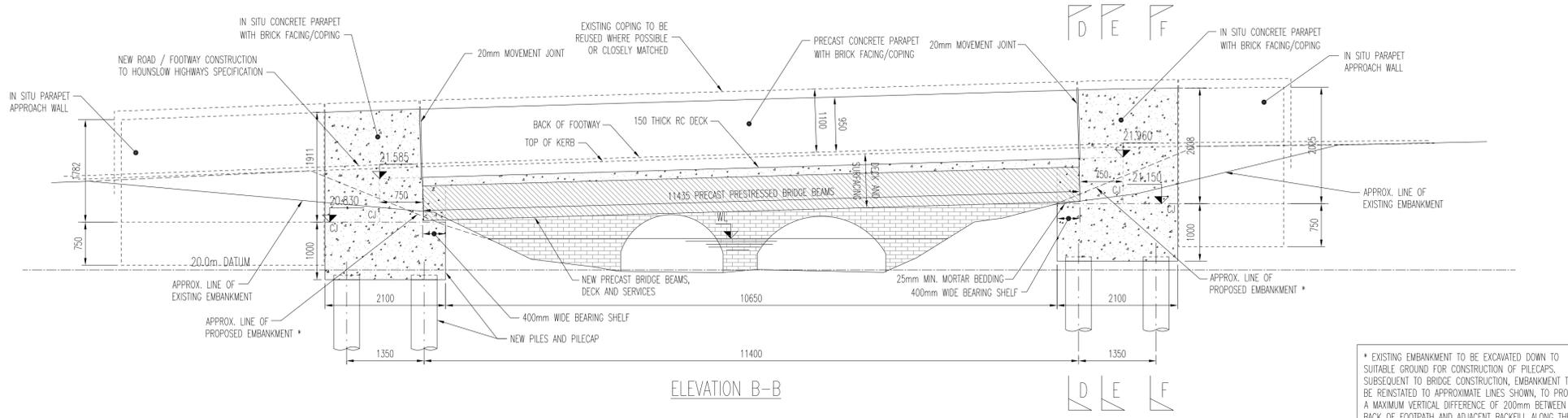
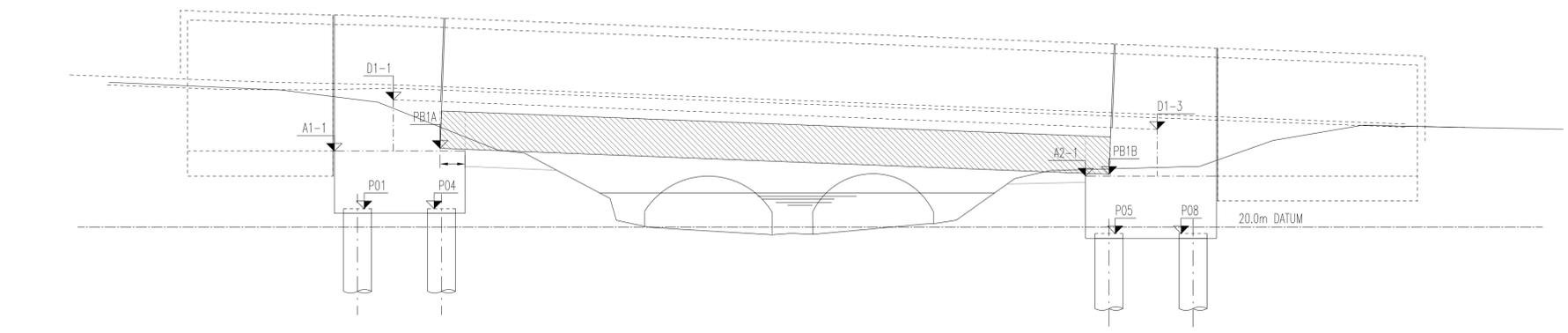


ELEVATION A-A

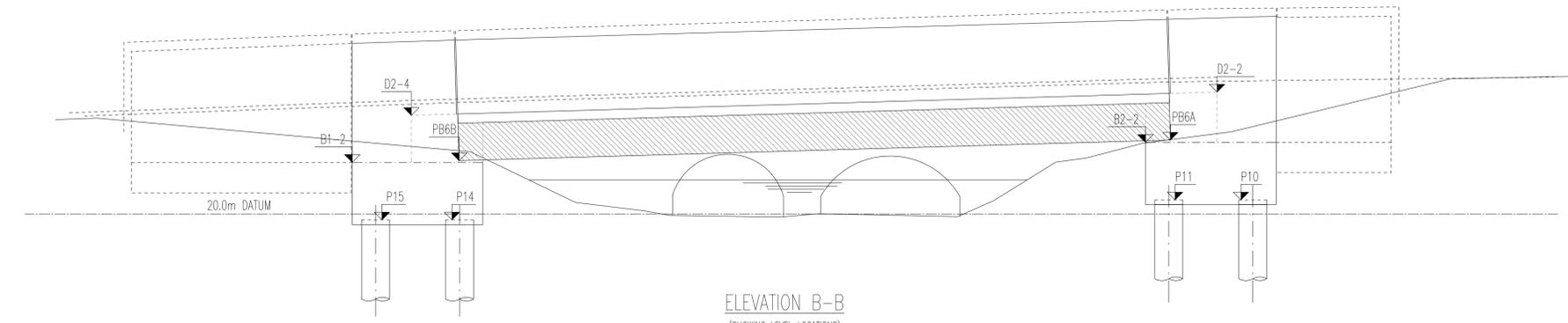
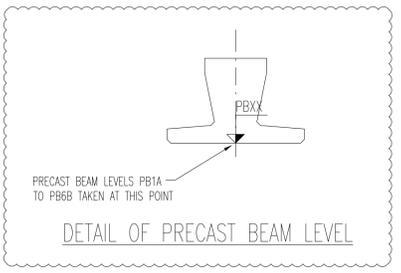


ELEVATION B-B

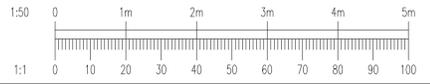
\* EXISTING EMBANKMENT TO BE EXCAVATED DOWN TO SUITABLE GROUND FOR CONSTRUCTION OF PILECAPS. SUBSEQUENT TO BRIDGE CONSTRUCTION, EMBANKMENT TO BE REINSTATED TO APPROXIMATE LINES SHOWN, TO PROVIDE A MAXIMUM VERTICAL DIFFERENCE OF 200mm BETWEEN BACK OF FOOTPATH AND ADJACENT BACKFILL ALONG THE LENGTH OF THE IN SITU PARAPET APPROACH WALLS, AND TO CONCEAL EXPOSED CONCRETE FROM THE IN SITU PILECAPS



ELEVATION A-A  
(SHOWING LEVEL LOCATIONS)



ELEVATION B-B  
(SHOWING LEVEL LOCATIONS)



NOTES:  
1. FOR GENERAL NOTES REFER TO DRAWING WIE-SA-04-0015

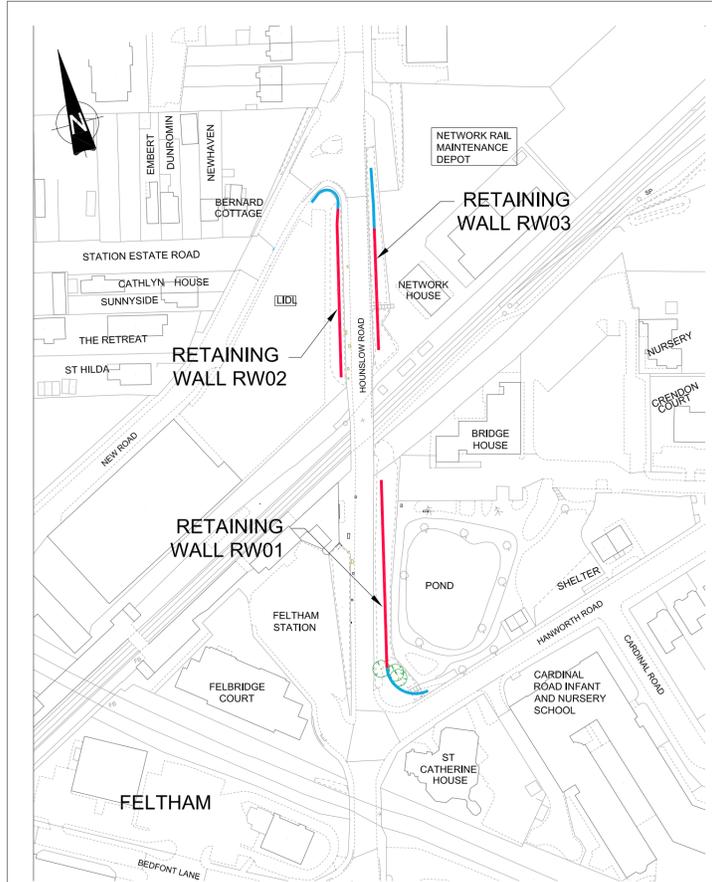
CO2	23.08.16	ADDITIONAL DETAIL AND SETTING OUT ADDED	SPO
CO1	19.08.16	ISSUED FOR TENDER	SPO
Rev	Date	Description	By

Project		<b>FELTHAM HIGH STREET</b>	
Title		<b>HOUNSLOW ROAD RIVER BRIDGE BRIDGE WIDENING GENERAL ARRANGEMENT ELEVATIONS</b>	
Client		<b>London Borough of Hounslow</b>	

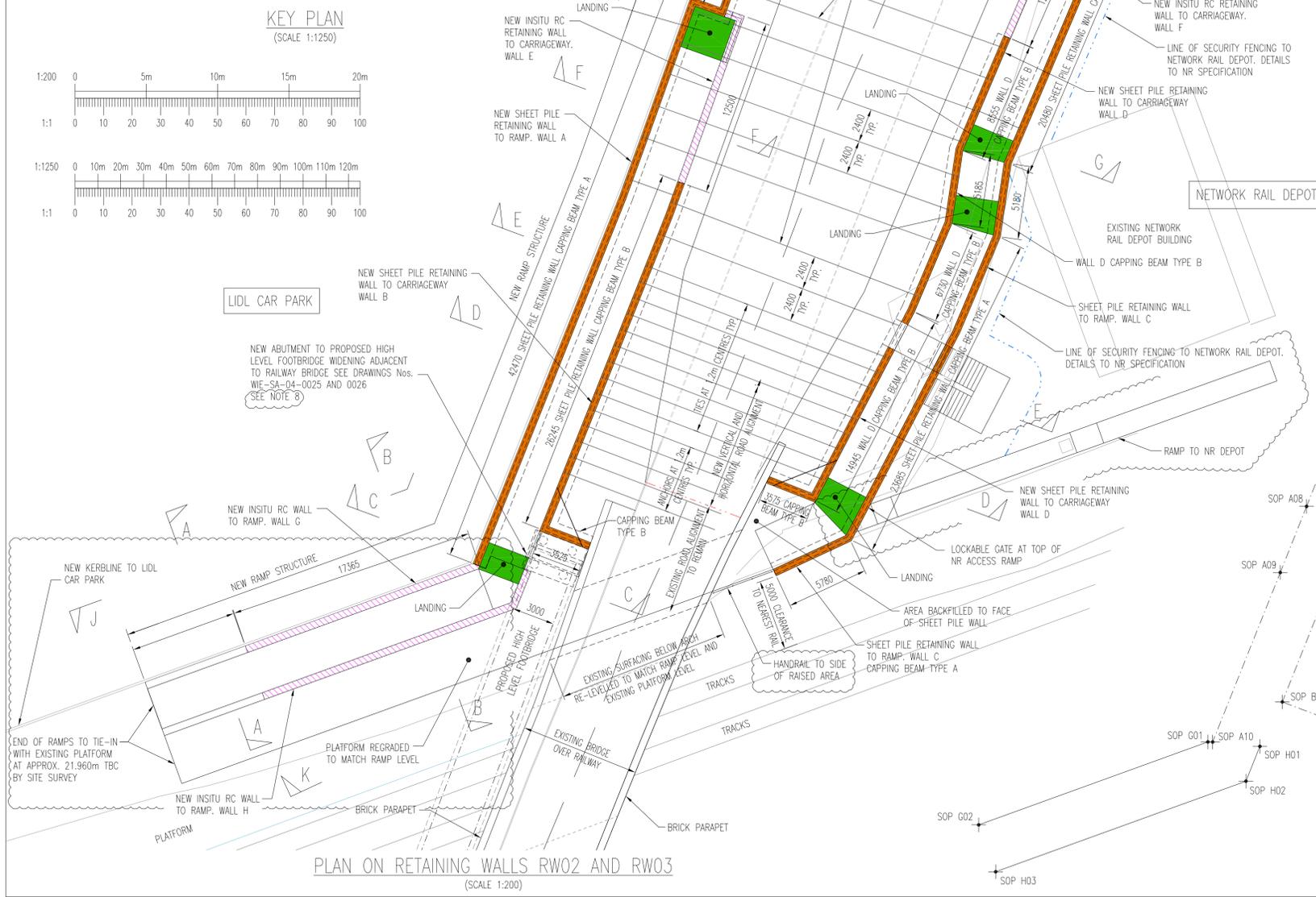
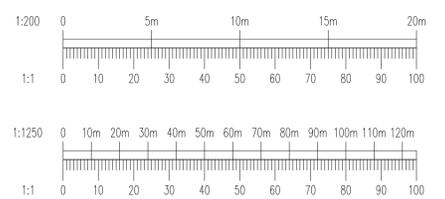
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mail@watermangroup.com www.watermangroup.com

Drawing Status				
<b>TENDER</b>				
Designed by	LF	Checked by	AAK	Project No
Drawn by	SPO	Date	AUGUST 2015	<b>CIV17189</b>
Scales @ A1		AS SHOWN		Computer File No
work to figured dimensions only				WIE-17189-SA-04-0015.dwg
Publisher	Zone	Category	Number	Revision
<b>WIE</b>	<b>SA</b>	<b>04</b>	<b>0016</b>	<b>C02</b>

File Path: K:\Projects\017189\_Feltham High St Bridge and Roadway\WIE-SA-04.dwg



KEY PLAN  
(SCALE 1:1250)



PLAN ON RETAINING WALLS RW02 AND RW03  
(SCALE 1:200)

SHEET PILE AND INSITU WALL SCHEDULE TABLE

SOP	EASTING	NORTHING	CUT OFF
A01	510720.734	173415.752	23.018
A02	510715.806	173409.771	23.473
A03	510712.744	173403.067	23.833
A04	510709.272	173393.321	24.396
A05	510707.199	173394.059	24.380
A06	510703.967	173384.988	24.199
A07	510702.373	173380.599	23.964
A08	510698.694	173371.356	23.468
A09	510696.846	173366.713	23.218
A10	510691.771	173354.823	22.587
B01	510706.468	173381.641	25.067
B02	510705.644	173379.354	25.182
B03	510701.946	173370.062	25.655
B04	510700.097	173365.416	25.900
B05	510696.990	173357.628	26.310
B06	510699.871	173356.535	26.310
C01	510736.978	173416.042	22.762
C02	510735.705	173410.623	22.981
C03	510734.440	173405.364	23.240
C04	510733.466	173403.076	23.385
C05	510736.686	173401.705	23.522
C06	510728.815	173383.206	22.724
C07	510728.052	173378.080	22.605
C08	510725.227	173371.412	22.253
C09	510717.730	173357.028	21.593
C10	510712.624	173354.649	21.361

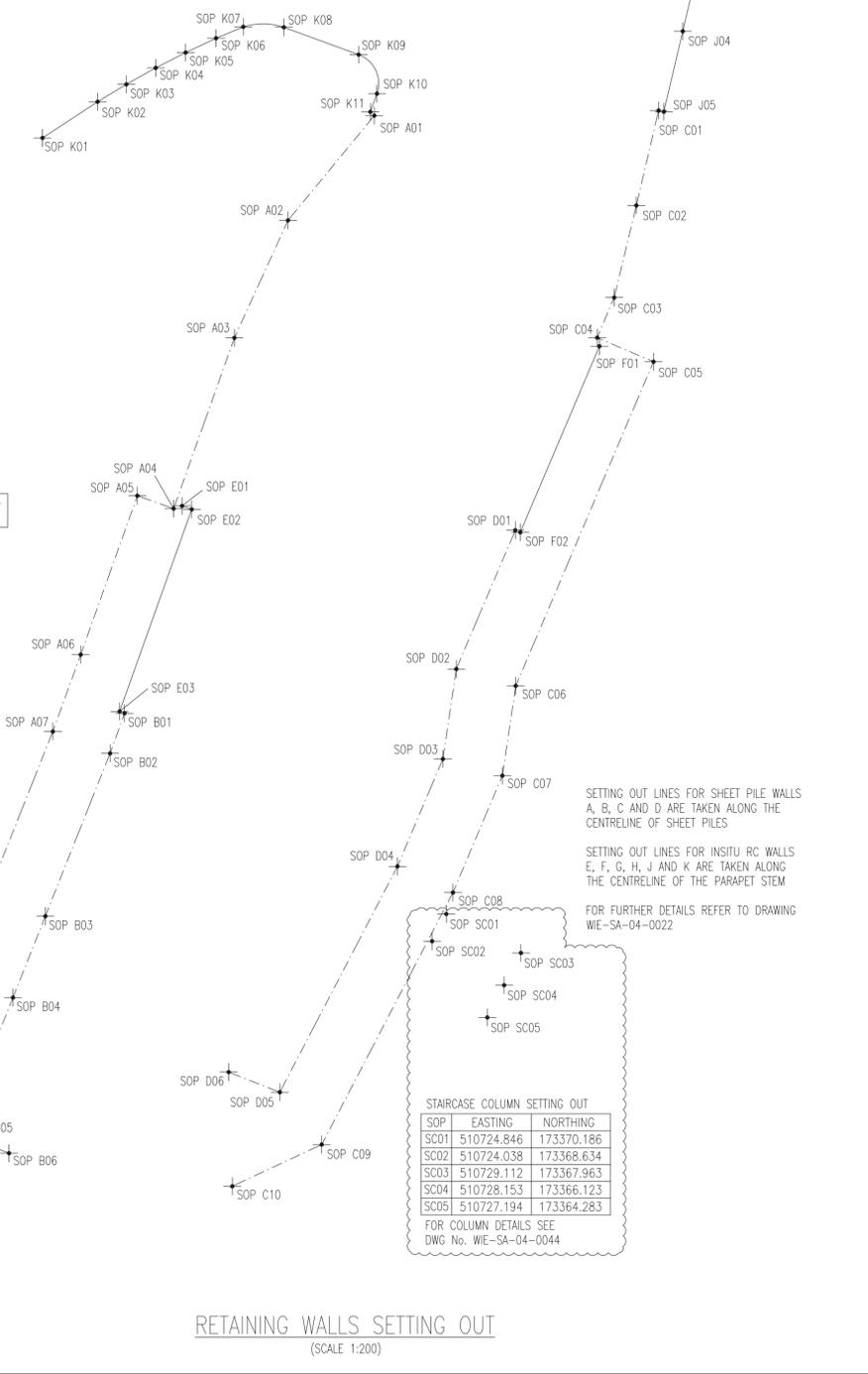
Eastings and Northings are in metres and relate to Ordnance Survey co-ordinates. Levels are in metres and relative to Ordnance Datum.

For further setting out information refer to details on drawing WIE-SA-04-0022

SHEET PILE AND INSITU WALL SCHEDULE TABLE

SOP	EASTING	NORTHING	CUT OFF
D01	510728.790	173392.087	24.052
D02	510725.419	173384.165	24.537
D03	510724.655	173379.036	24.823
D04	510722.059	173372.905	25.213
D05	510715.340	173360.014	25.925
D06	510712.415	173361.164	25.845
E01	510709.752	173393.477	N/A
E02	510710.309	173393.279	N/A
E03	510706.183	173381.743	N/A
F01	510733.593	173402.587	N/A
F02	510729.074	173391.967	N/A
G01	510692.099	173354.813	N/A
G02	510675.680	173348.998	N/A
H01	510695.417	173354.509	N/A
H02	510694.439	173352.066	N/A
H03	510676.871	173345.707	N/A
J01	510745.922	173441.195	N/A
J02	510741.292	173429.784	N/A
J03	510739.461	173425.269	N/A
J04	510738.357	173420.566	N/A
J05	510737.278	173415.972	N/A
K01	510701.783	173414.476	N/A
K02	510704.932	173416.539	N/A
K03	510706.575	173417.535	N/A
K04	510708.252	173418.473	N/A
K05	510709.960	173419.351	N/A
K06	510711.698	173420.168	N/A
K07	510713.252	173420.816	N/A
K08	510715.569	173420.792	N/A
K09	510719.838	173419.232	N/A
K10	510720.885	173417.001	N/A
K11	510720.509	173415.965	N/A

Eastings and Northings are in metres and relate to Ordnance Survey co-ordinates. Levels are in metres and relative to Ordnance Datum.



RETAINING WALLS SETTING OUT  
(SCALE 1:200)

STAIRCASE COLUMN SETTING OUT

SOP	EASTING	NORTHING
SC01	510724.846	173370.186
SC02	510724.038	173368.634
SC03	510729.112	173367.963
SC04	510728.153	173366.123
SC05	510727.194	173364.283

FOR COLUMN DETAILS SEE DWG No. WIE-SA-04-0044

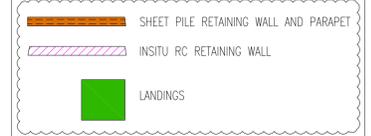
- NOTES:
- All dimensions are in millimetres unless noted otherwise
  - All levels are in metres and relate to Ordnance Datum
  - Details shown on these drawings are based on currently available GI information and are subject to change following receipt of final GI report
  - Maximum allowable sheet pile deflection to be 20mm
  - Setting out, levels and dimensions on these drawings are based on Topographical Surveys by Hounslow Highways, drawing nos:
    - SDG 14146-14-01 with Network Rail and LBH Additions 2015 REV 1 received 16 July 2015
    - A0609814 FIC Topo Survey - Lidl & NR Compound 2015-07-03 REV 1 received 16 July 2015
    - '3d proposed ramp and stairs' received 07 Sept 2015
  - Joints in insitu RC walls and brick facings to be provided at suitable locations
  - New brick finishes to retaining walls and ramps to match existing Rail Bridge bricks as closely as possible
  - For concept design of pedestrian footbridge see drawing no. WIE-SA-04-0033

SAFETY, HEALTH & ENVIRONMENTAL INFORMATION  
UNUSUAL SIGNIFICANT HAZARDS

- It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement. In addition to the hazards/risks normally associated with the types of work detailed on this drawing, note the following:
- CONSTRUCTION
- Working near excavations
  - Presence of buried services
  - Installation of ties
  - Risk of piling falling on railway during pitching.
  - Sheet piling hitting existing bridge foundations.
  - Noise and vibration associated with driving piles.

- MAINTENANCE / CLEANING
- Working near an operational depot/service yard.
  - Risks associate with carrying out repairs on a tied structure.
  - Future service excavation hitting anchors.
  - Possible requirement for maintenance painting of finishing

- DECOMMISSIONING / DEMOLITION
- Stability of wall in case of accidental removal of tie



Rev	Date	Description	By
E02	25.07.16	HOUNSLOW HIGHWAYS COMMENTS INCORPORATED	SPO
E01	30.06.16	ISSUED FOR CONSTRUCTION	SPO
C02	26.04.16	WALL LENGTH, SETTING OUT AND LANDING REVISED	SPO
C01	05.04.16	ISSUED FOR TENDER	SPO
A05	17.12.15	SHEET PILE AND INSITU WALL LENGTHS REVISED, BRIDGE ABUTMENT RELOCATED	SPO
A04	11.11.15	ISSUED FOR AP RAMP UNDER ARCH REVISED	SPO
A03	26.10.15	SHEET PILE WALLS REPLACE INSITU WALLS TO RAMP. NOTES AND DIMENSIONS ADDED	SPO
A02	22.10.15	ISSUED FOR COMMENT. NOTES ADDED, SHEET PILE LENGTH REVISED	SPO
A01	01.10.15	FIRST ISSUE FOR COMMENT	SPO

Project  
**FELTHAM HIGH STREET**

Title  
**RETAINING WALLS Nos. RW02 AND RW03 AND RAMP STRUCTURES GENERAL ARRANGEMENT PLAN**

Client  
**London Borough of Hounslow**

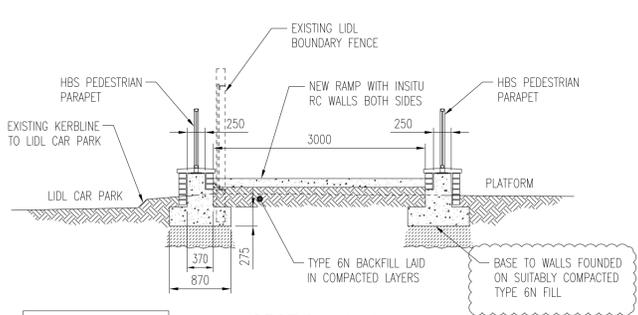


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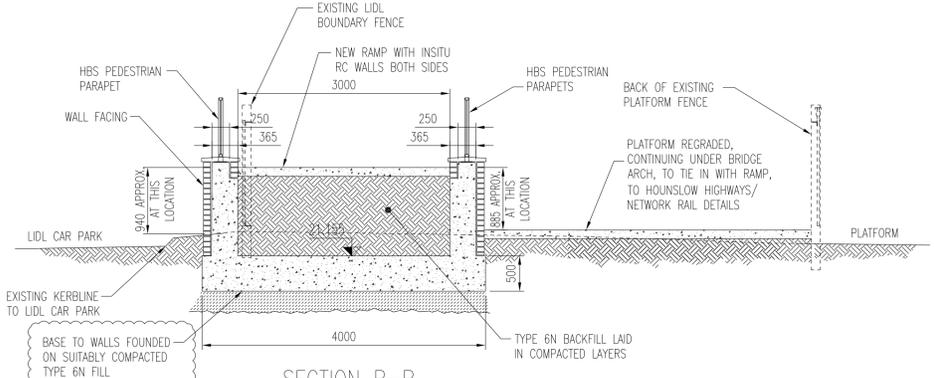
Drawing Status  
**CONSTRUCTION**

Designed by	VB	Checked by	AAK	Project No.	CIV17189
Drawn by	SPO	Date	SEPT 2015	Computer File No.	WIE-17189-SA-04-0020.dwg
Scales @ A1	work to figured dimensions only		1:200 1:1250	Revision	

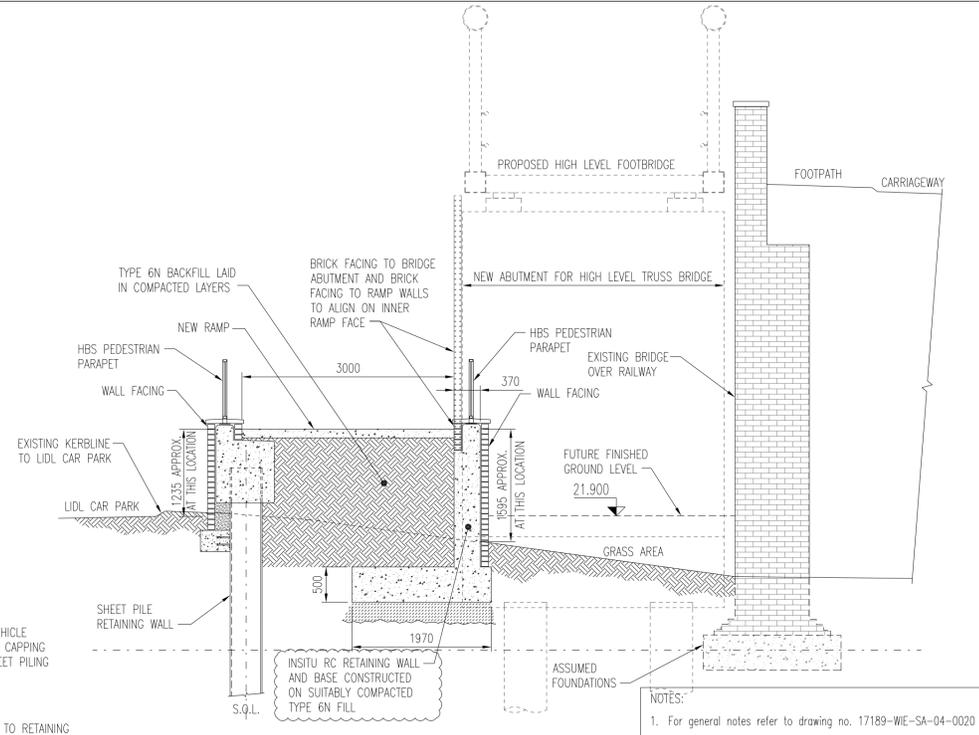
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WIE	SA	04	0020	E02



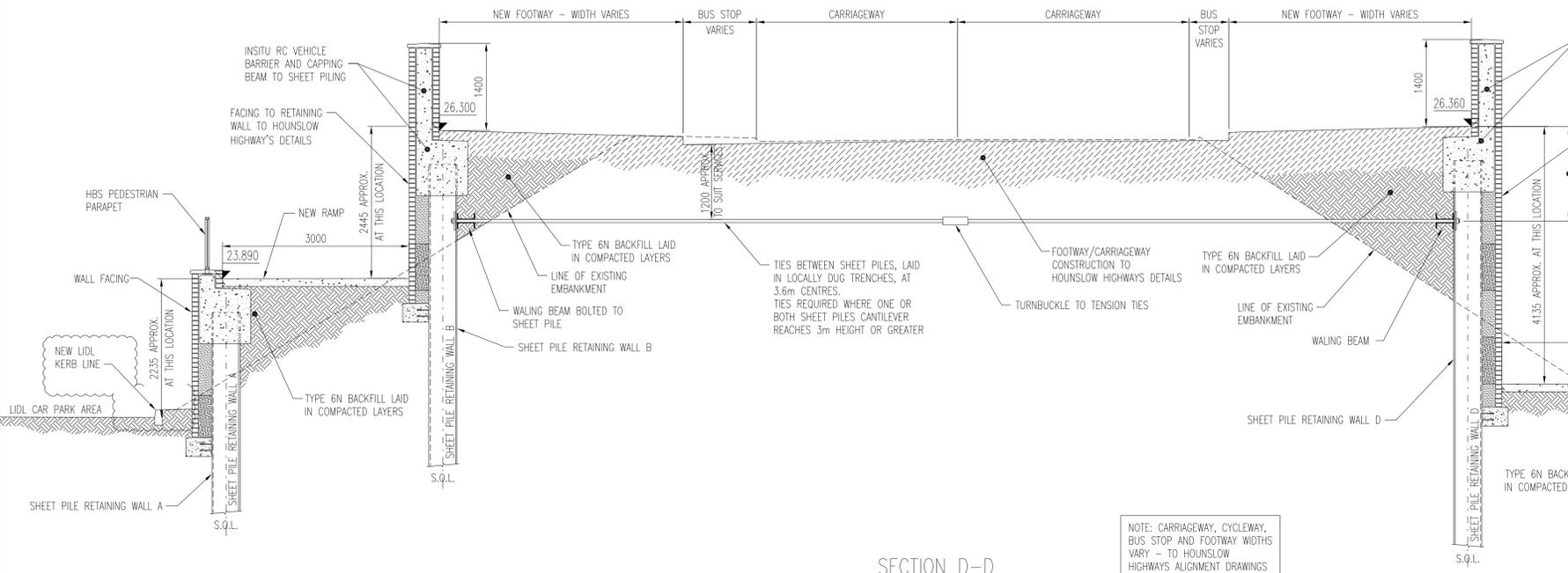
SECTION A-A



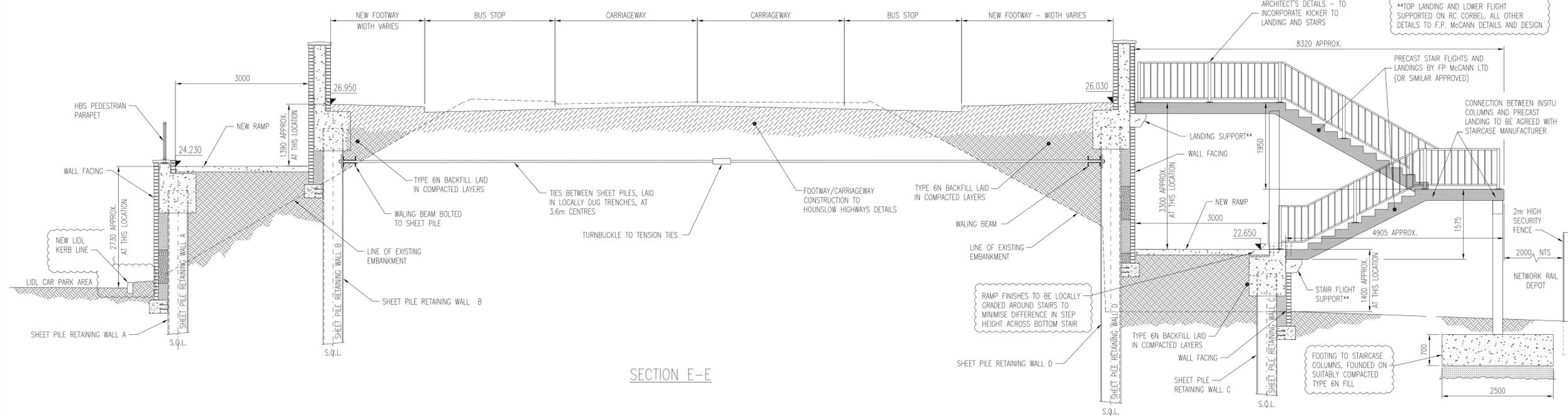
SECTION B-B



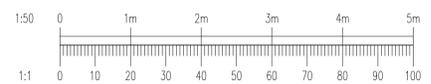
SECTION C-C



SECTION D-D



SECTION E-E



NOTES:  
1. For general notes refer to drawing no. 17189-WIE-SA-04-0020

**SAFETY, HEALTH & ENVIRONMENTAL INFORMATION  
UNUSUAL SIGNIFICANT HAZARDS**

It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement. In addition to the hazards/risks normally associated with the types of work detailed on this drawing, note the following:

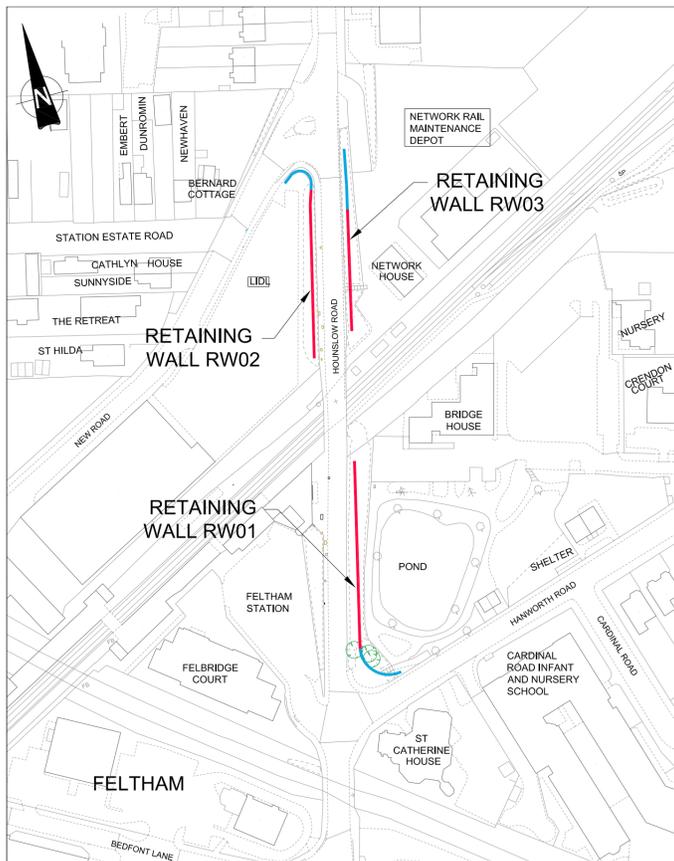
- CONSTRUCTION**
- Working near excavations
  - Presence of buried services
  - Installation of ties
  - Risk of piling falling on railway during piling.
  - Sheet piling hitting existing bridge foundations.
  - Noise and vibration associated with driving piles.
  - Working near existing buildings
- MAINTENANCE / CLEANING**
- Working near an operational depot/service yard.
  - Risks associate with carrying out repairs on a tied structure.
  - Future service excavation hitting anchors.
  - Possible requirement for maintenance painting of finishing
- DECOMMISSIONING / DEMOLITION**
- Stability of wall in case of accidental removal of tie

Rev	Date	Description	By
E02	25.07.16	HOUNSLOW HIGHWAYS COMMENTS INCORPORATED	SPO
E01	30.06.16	ISSUED FOR CONSTRUCTION	SPO
C02	26.04.16	RAMP AND FOOTPATH LEVELS ADDED	SPO
C01	05.04.16	ISSUED FOR TENDER	SPO
A06	03.02.16	STAIRCASE REVISED	SPO
A05	25.01.16	SECTION A-A AND B-B FENCES ADDED	SPO
A04	17.12.15	SHEET PILE, CAPPING BEAM AND VEHICLE BARRIER REVISED TO ARCHITECT'S COMMENTS. SECTION C-C REVISED	SPO
A03	11.11.15	ISSUED FOR AIP SECURITY FENCE RELOCATED. KERB LINES ADDED	SPO

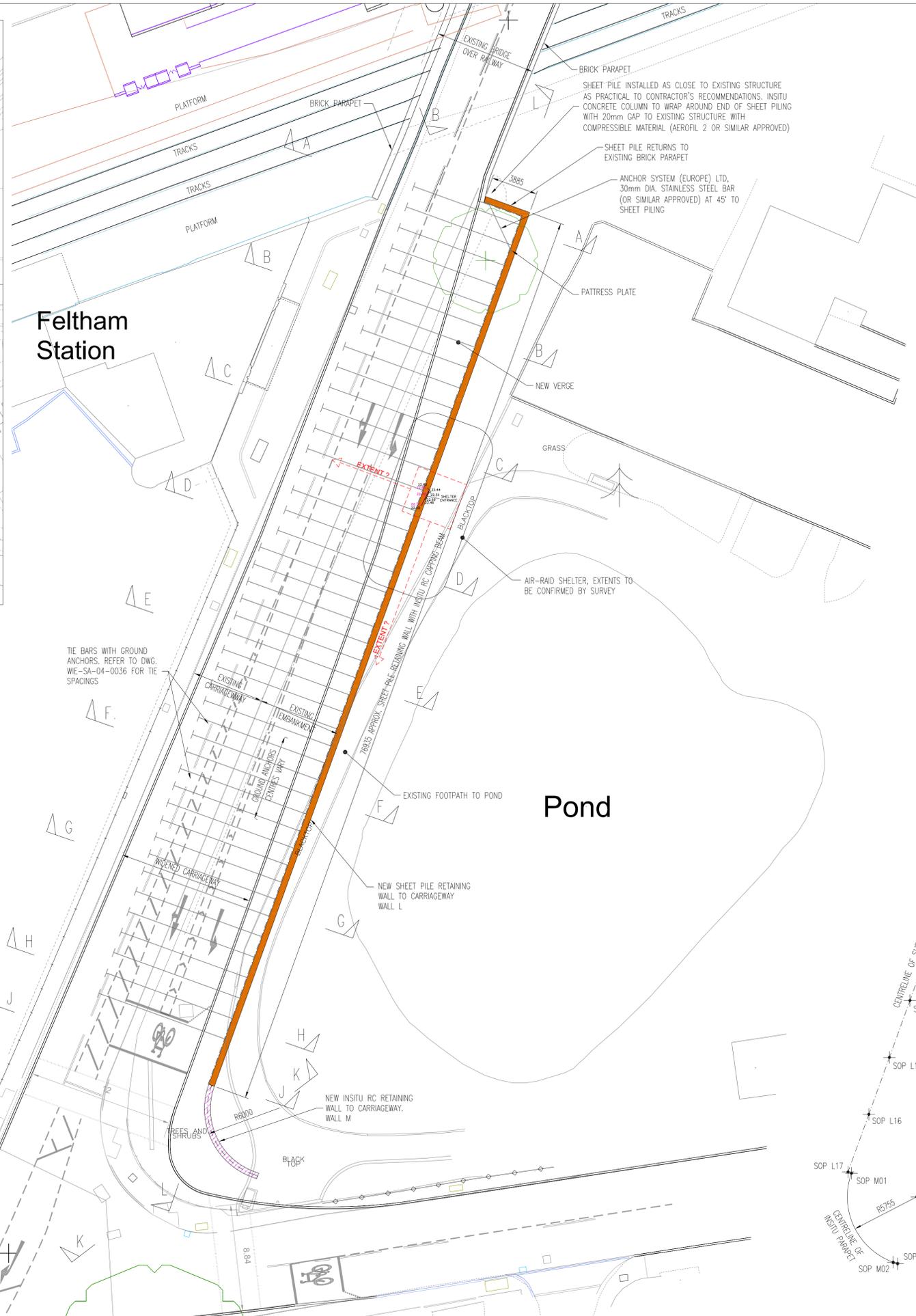
**FELTHAM HIGH STREET**  
RETAINING WALLS Nos. RW02 AND RW03  
AND RAMP STRUCTURES  
GENERAL ARRANGEMENT  
CROSS SECTIONS SHEET 1 OF 2



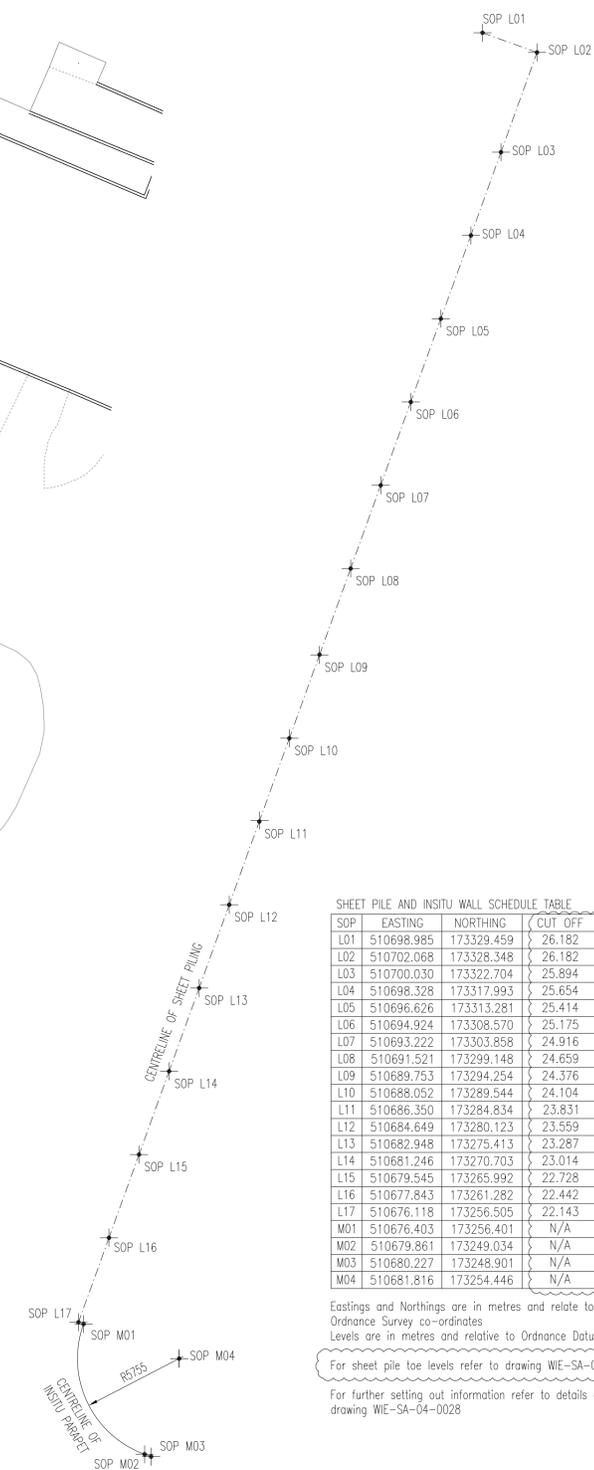
Pickfords Wharf Clink Street London SE1 9DG				
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mail@watermangroup.com www.watermangroup.com				
<b>CONSTRUCTION</b>				
Designed by	VB	Checked by	AAK	Project No
Drawn by	SPO	Date	SEPT 2015	CIV17189
Scales @ A1 work to figured dimensions only		1:200	Computer File No: WIE-17189-SA-04-0021.dwg	
Publisher	Zone	Category	Number	Revision
WIE	SA	04	0021	E02



KEY PLAN  
(SCALE 1:1250)



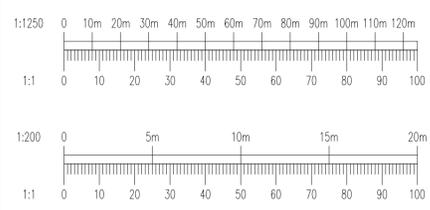
PLAN ON RETAINING WALL RW01  
(SCALE 1:200)



SHEET PILE AND INSITU WALL SCHEDULE TABLE

SOP	EASTING	NORTHING	CUT OFF
L01	510698.985	173329.459	26.182
L02	510702.068	173328.348	26.182
L03	510700.030	173322.704	25.894
L04	510698.328	173317.993	25.654
L05	510696.626	173313.281	25.414
L06	510694.924	173308.570	25.175
L07	510693.222	173303.858	24.916
L08	510691.521	173299.148	24.659
L09	510689.753	173294.254	24.376
L10	510688.052	173289.544	24.104
L11	510686.350	173284.834	23.831
L12	510684.649	173280.123	23.559
L13	510682.948	173275.413	23.287
L14	510681.246	173270.703	23.014
L15	510679.545	173265.992	22.728
L16	510677.843	173261.282	22.442
L17	510676.118	173256.505	22.143
M01	510676.403	173256.401	N/A
M02	510679.861	173249.034	N/A
M03	510680.227	173248.901	N/A
M04	510681.816	173254.446	N/A

Eastings and Northings are in metres and relate to Ordnance Survey co-ordinates  
Levels are in metres and relative to Ordnance Datum  
For sheet pile toe levels refer to drawing WIE-SA-04-0036  
For further setting out information refer to details on drawing WIE-SA-04-0028



- NOTES:
- All dimensions are in millimetres unless noted otherwise
  - All levels are in metres and relate to Ordnance Datum
  - Maximum allowable sheet pile deflection to be 20mm (during all stages of construction)
  - Setting out, levels and dimensions on these drawings are based on Topographical Surveys by Hounslow Highways, drawing nos:
    - SDC 14146-14-01 with Network Rail and LBH Additions 2015 REV 1 received 16 July 2015
  - Movement joints in capping beams, parapets, insitu RC walls and brick facings to be at nom. 12m centres
  - New brick finishes to retaining wall to be as follows:
    - Traffic face (carriageway side) to match existing Rail Bridge bricks as closely as possible
    - Pond side face to match existing brick retaining wall adjacent to pond footpath as closely as possible

**SAFETY, HEALTH & ENVIRONMENTAL INFORMATION**  
**UNUSUAL SIGNIFICANT HAZARDS**

- It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement. In addition to the hazards/risks normally associated with the types of work detailed on this drawing, note the following:
- CONSTRUCTION**
- Working near excavations
  - Presence of buried services
  - Installation of ties
  - Risk of piling falling on railway during pitching.
  - Sheet piling hitting existing bridge foundations.
  - Noise and vibration associated with driving piles.
  - Driven anchors hitting services.
  - Stability of existing embankment
- MAINTENANCE / CLEANING**
- Working near an operational depot/service yard.
  - Risks associate with carrying out repairs on a tied structure.
  - Future service excavation hitting anchors.
  - Possible requirement for maintenance painting of finishing
- DECOMMISSIONING / DEMOLITION**
- Stability of wall in case of accidental removal of tie

Rev	Date	Description	By
E04	???.???	PILE CUT-OFF LEVELS AND NOTE ADDED	SPO
E03	29.07.16	NOTE REVISED	SPO
E02	25.07.16	HOUNSLOW HIGHWAYS COMMENTS INCORPORATED	SPO
E01	01.07.16	ISSUED FOR CONSTRUCTION	SPO
C01	05.04.16	ISSUED FOR TENDER	SPO
A05	15.12.15	MINOR REVISIONS TO NOTES	SPO
A04	06.11.15	ISSUED FOR AIP GROUND ANCHOR SPACINGS REDUCED AND ADDITIONAL ANCHORS ADDED TO PILING CONTRACTOR'S DESIGN	SPO
A03	26.10.15	WALL REVISED AT HANWORTH ROAD JUNCTION TO HOUNSLOW HIGHWAYS REALIGNMENT	SPO
A02	22.10.15	ISSUED FOR COMMENTS SHEET PILE WALL REALIGNED TO HOUNSLOW HIGHWAYS REALIGNMENT	SPO
A01	07.10.15	FIRST ISSUE FOR COMMENT	SPO

**Project**  
**FELTHAM HIGH STREET**

**Title**  
**RETAINING WALL No. RW01**  
**GENERAL ARRANGEMENT PLAN**

**Client**  
**London Borough of Hounslow**

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Drawing Status: **CONSTRUCTION**

Designed by: VB	Checked by: AAK	Project No: CIV17189
Drawn by: SPO	Date: SEPT 2015	Computer File No: WIE-17189-SA-04-0027.dwg
Scales @ A1 work to figured dimensions only: 1:200 1:1250		
Publisher: WIE	Zone: SA	Category: 04
Number: 0027	Revision: E04	