



Document History (Continued):

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1 0	14/11/16	██████████	██████████	██████████	For CRL Acceptance

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Executive Summary

PMI00491 was issued on 08th October 2015 and at subsequent contract progress meetings the difficulties of taking forward the design were discussed. Amongst the issues discussed was the location and state of the remaining cellars below the C422 External Works.

This report has been produced to determine what cellars remain and what challenges they may create for the design. No reference has been made to the Undertakings and Assurances that may have been given regarding the cellars; this being beyond the scope of the report. It was also decided not to engage a structural engineer at this stage, but consider this option once it is known what cellars remain.

On this basis, the report has been produced by experienced infrastructure engineers with a basic knowledge of structures. Thus, the background work of establishing the current position regarding cellars has been completed. We now recommend a review of the report by structural and construction methodology engineers. Also whilst various physical surveys were progressed as the Crossrail Act was made, C422 are informed by CRL that no pre-construction condition surveys exist for the cellars

Furthermore, previous contracts have not placed adequate backfill within the cellars that have been abandoned. This raises concerns about the ability to support public highway above and recommendations on the way forward for the pavement design are made.

Lastly, during report production, drawings showing grout shaft details were received. Whilst their ability to support public highway is not in question, they complicate pavement design details and recommendations on this topic are made.

1 Introduction

1.1 Purpose of the Document

Historically within London, cellars have been built below the public highway and have existed like this for hundreds of years. Crossrail construction methods have used diaphragm walling to sink ticket halls within the plots of land acquired. This walling method has resulted in cellars being backfilled and abandoned in areas beside the main ticket halls. Elsewhere cellars have been un-affected.

C422's task is to design the external works surrounding the Tottenham Court Road Station. To complete this task an understanding of what cellars remain and the way they potentially impact upon the design is needed. The method of report production and its objectives are therefore to:

1. Gather information relating to the existing cellars
2. Produce a schedule of all cellars
3. Ascertain what cellars remain and produce schedule of them
4. Document any issues associated with the remaining cellars
5. Produce recommendations on the way forward.

It must also be noted that the report has been produced by infrastructure engineers without input from structural or construction methodology engineers. This approach was agreed with the Employer and CRL at various progress meetings.

Station works designed by C134 can broadly be put into two categories in relation to the cellars.

- Structural works, comprising enabling works demolition, backfilling and diaphragm walling. [Now completed]
- External civils works, comprising construction of highways, drainage and street lighting. [Yet to be constructed]

The location plan for the C422 External Works is contained at **Appendix A**.

The Consultant first requested the information regarding the presence of cellars and the standard of backfill compaction from the Employer within RFI No. DTR408, raised on 9 October 2014.

Areas within the hoarding have been used for a variety of construction activities and along Great Chapel Street these have been directly beside or above retained cellars. Consideration of the impact of 130 tonne piling rigs adjacent to retained cellars is beyond the scope of this report.

1.2 Information relating to existing cellars

To resolve what happened during the structural works, recourse has been made to the Health and Safety Files (HSF) produced for the various Crossrail contracts. A CDM obligation is that the H&S Files contain the "as-built" information. Please refer to **Table 1**, that details the H&S Files provided:



C422 - Tottenham Court Road Station -Stage D+ Report on Cellars
 C422-LAO-C-RGN-N105_WS089-50005
 Rev 1.0

ID	File Name	Document Title	Document Type	Any References to Basements	3D Survey Included?
	C208 MCG OI STP N1 05 50002	C208 Tottenham Court Road, Western Ticket Hall Health and Safety File	Health and Safety File	Yes	No
2	C134 LU OI LRC-N105_WS093 50001	Health and Safety File for the Goslett Yard Box and Escalator Decline	Health and Safety File	No	No
3	C300-BFK-Z7-STP-CRTOO_SDOS-5004+ Version 2	C300/410 Western Tunnels & Caverns Project Health and Safety File	Health and Safety File	No	No
4	C300-BFK-Z7-STP-CRTOO_ST005-50041 • Version 3	C300/410 Western Tunnels & Caverns Project Health and Safety File	Health and Safety File	No	No
5	C421-8BMC-RGN-N105_WS089-50115	C421 Tottenham Court Road Western Ticket Hall Health & Safety File Grout Shaft 7	Health and Safety File	No	No
6	C421-BBMZ7 RGN-N105_V6089-50005	C421 Tottenham Court Road Western Ticket Hall Health & Safety File	Health and Safety File	No	No
7	C422-1000-COM-108 HSF file req LOR to CRL151204	C422 Tottenham Court Road Health and Safety Information	Health and Safety File	No	No

Table 1. Health & Safety Documents reviewed for cellar/basement information.

A full schedule of all information provided to determine the presence of the cellars is provided within **Appendix B**. Documents obtained include the above Health & Safety Files, cellar/basement survey drawings and information provided with responses to Early Warning Notices raised.

Documentation received from C134 for the external works contained a General Arrangement drawing C134-OVE-D-DDA-N105_1-00201(C01) which detailed the cellar locations at the Western Ticket Hall only, the drawing is located within **Appendix B**. No C134 drawing of existing cellar topographic surveys at Goslett Yard have been made available. C422 are informed that no pre-construction condition surveys were undertaken but access was gained and a 3D survey undertaken for most of the cellars. The 3D surveys were not contained in the C134 Works Information, but obtained from ProjectWise. It must be noted however that little information has been provided on cellars at the south end of Dean Street around the junction to Carlisle Street.

The cellars constitute part of the property freehold. They are designed to carry a highway loading appropriate at the time of construction. Maintenance of the cellar, including waterproofing is the responsibility of the freeholder, whereas Westminster City Council (WCC) does not take any responsibility for their upkeep.

This document was issued on A Site as Revision 0.1 on the 20th of April 2016. No comments on the draft report issued in April have been received.

2 Pre-construction conditions

Sketches WSP-C422-SK-001 and 002 identify the locations of the existing cellars prior to the project commencing. Located within **Appendix C** the existing cellar locations prior to construction work have been used to produce the cellar schedule. The numbering is shown on the sketches.

2.1 Schedule of all cellars

Each cellar within and close to the site extents has been allocated a unique identification number which coincides with the cellar schedule. The full schedule of all the cellars identified is included within **Appendix D**. The cellar schedule has been divided up into each individual street with comments provided where additional information has been made available.

Referencing the 3D cellar survey provided by CRL, a typical cross section of the existing cellar arrangement is detailed below, **Figure 1**. This is the anticipated cellar arrangement found at the WTH and ETH sites.

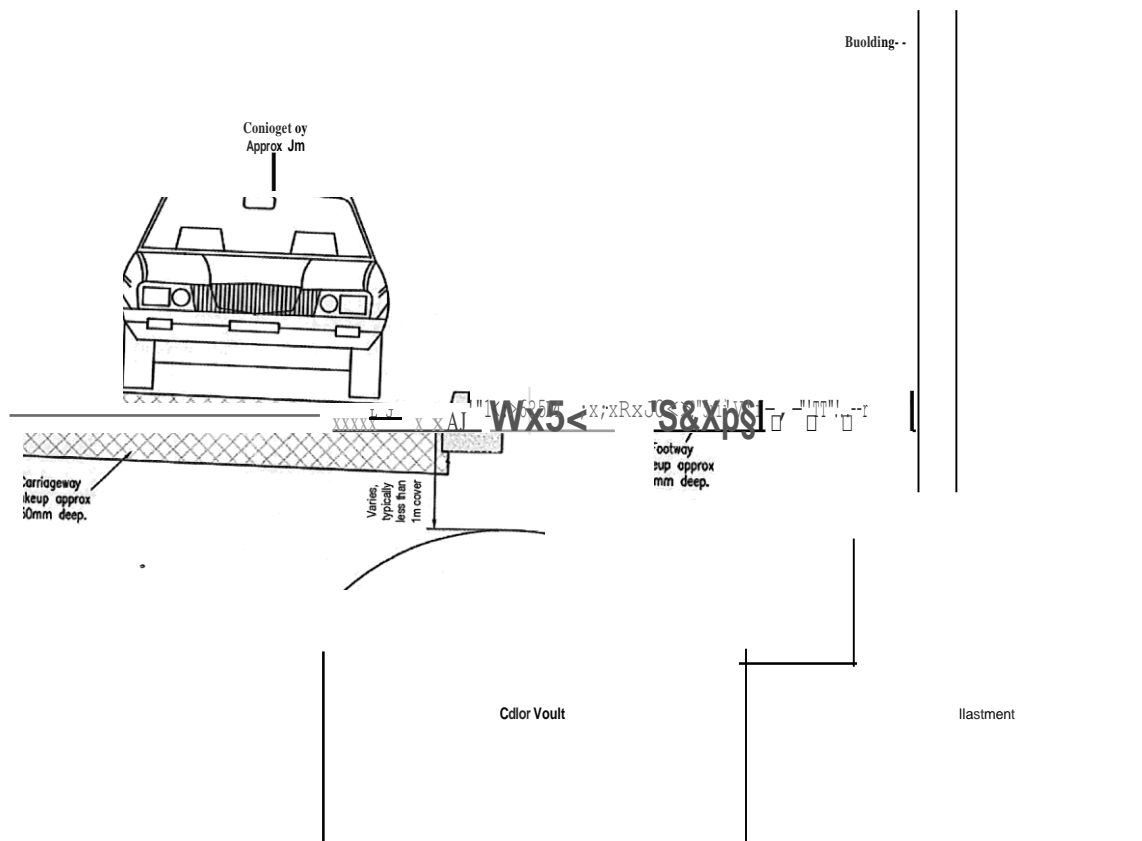


Figure 1. Typical Cross Section of Existing Cellar Arrangement

3 Construction Interaction

3.1 Construction Work affecting Cellars

The cellars located around the perimeter of the WTH (north and south boxes) have all been incorporated into the proposed construction works. The WTH is constructed using the diaphragm wall method. This has resulted in the removal of the basements and cellars directly beside the walling which should have been backfilled with compacted granular material to pile mat formation level prior to the walling works. The following HSF "C208-MCG-01-STP-N105-50002, C208 Tottenham Court Road - Western Ticket Hall Health and Safety File" has been referenced to determine what construction work has been undertaken for the cellars.

There was no mention of cellars or basements in the following two H&S files:

- C300-BFK-Z7-STP-CRTO0_ST005-50041 -Version 2
- C421-BBM-Z7-RGN-N105_WS089-50005

These files only mention diaphragm walls in relation to tunnelling, and aspects other than basements or cellars.

With regards to the backfilling of redundant cellars, **Figure 2.** provides a typical cross-section including the interaction with the diaphragm wall. Typically the ground will have been cleared of all material up to 1.8m from the outside edge of the diaphragm wall prior to install.

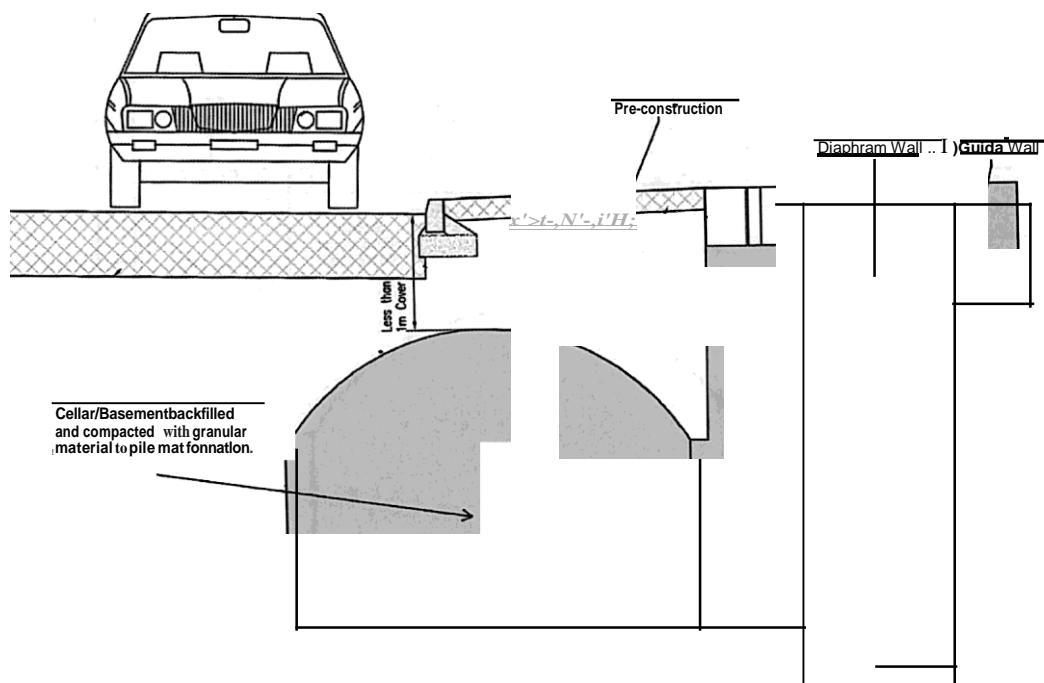


Figure 2. Cross Section of typical cellar post construction.

A schedule has been created to detail the works undertaken on each of the cellars and from which HSF document the information originated. **Figure 3.** below provides an example of the schedule for the cellars filled in on Dean Street.

Side of Road	Dean, Street	Filled in Y/N?	Information
West	DS_01	y	According to file C208-MCG-01-SJ-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_002	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
west	D5_003	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	05_004	Y	According to file C208-MCG-01aSTP-1\1105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_005	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.

Figure 3. Schedule of Cellars Filled in on Dean Street

Also detailed in **Appendix B** is an EWN from the Employer stating that during the initial phase of works, a proportion of the cellars along Dean Street had not been filled in with suitable material. There is a concern that these voids could affect the urban realm works and the quality and durability of the architectural pavement adjacent to the ground floor ticket hall entrance in particular.

3.2 Presence of Grout Shafts

Whilst the primary focus of this report is cellars, it is apparent from the detailed grout shaft drawings received on 9 August 2016 that these temporary structures could also impact upon the pavement design. **Appendix E** contains these drawings referencing a grout shaft located at the northern end of Dean Street.

From other drawings, there is also an indication that there are further grout shafts located within Goslett Yard and on Sheraton Street adjacent to Great Chapel Street turn. The latter is located on the boundary of the works, but may still impact the works.

At present these temporary structures are not within the scope of this report, therefore it is recommended that the Employer instruct the Consultant to investigate further the impact of these shafts on the design.



4 Present Condition

The remaining cellars left after the construction works will fall into two categories:

- Cellars which have remained untouched throughout the construction works
- Cellars that have been abandoned in the course of the works

All remaining cellars raise a further issue that there is no structural information present to determine their condition prior to and during construction works. Without this information on these cellars, the possible changes to the highway loadings can't be fully assessed.

Appendix F contains a full schedule of cellars that remain untouched and a schedule for cellars that have been filled in.

A full review has been completed to identify if existing cellars clash with the proposed drainage and utility infrastructure. This is reported in document C422-LAO-U-RGN-N105_WS089-50001 Stage D+ Report on Infrastructure/ Utility Clashes.

WCC have been contacted to determine if they have any historic survey information regarding the existing cellars that remain. Proposed drawings detailing the drainage design have been issued to WCC for review and comment. The response received from WCC regarding the cellar at Hollen Street is located within **Appendix G**.

4.1 Issues associated with the remaining cellars

The existing cellar at the junction of Hollen Street/Great Chapel Street requires reviewing in further detail. With proposed changes to the kerblines at the junction location, the tracking analysis for the 10.5m refuse vehicle and the fire tender have highlighted there is an over-run above the cellar footprint. A number of options are available as follows;

1. Remove the footway to the north of Hollen Street which would create additional carriageway for vehicle manoeuvres. This option would still be within the highway boundary
2. Strengthen the cellar to accommodate larger vehicles.
3. Restrict larger vehicles from using this route; however this is the least favoured option due to Hollen Street providing the gateway into the area for servicing the commercial units.

A more detailed review has determined that the Pumping Appliance can avoid the cellar. This has been achieved following Option 1. above. **Figure 4.** on the following page, as taken from drawing C422-LAO-D-DDL-N105_1-10035(P04) contained in **Appendix H**, highlights this case. However the kerbs within the C422 design remain as per the C134 drawings and no instruction has been received to alter the layout

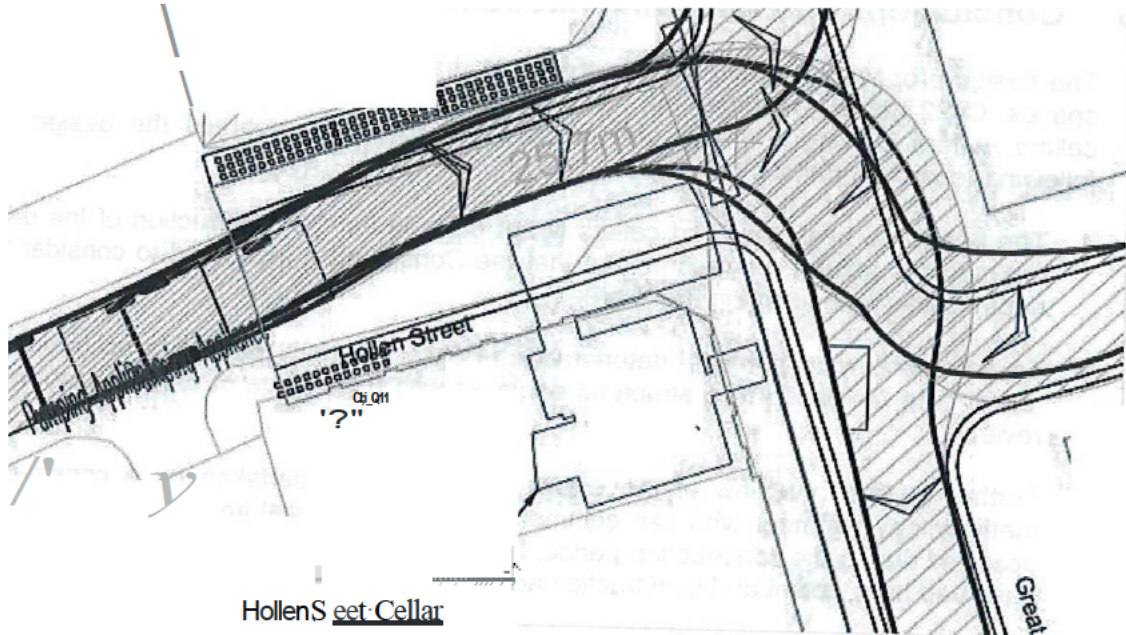


Figure 4. Vehicle swept path for Hollen Street Junction adjacent to an existing cellar

Whilst there are only a fraction of cellars beneath the existing and/or proposed carriageways, the live loading of vehicles will not be vertical in nature, with a likely zone of influence of 22.5° to 45° away from the edge of the wheel track. This also applies to areas within the hoarding that have been used for a variety of construction activities including movement of heavy piling rigs as indicated in Figure 5. below.

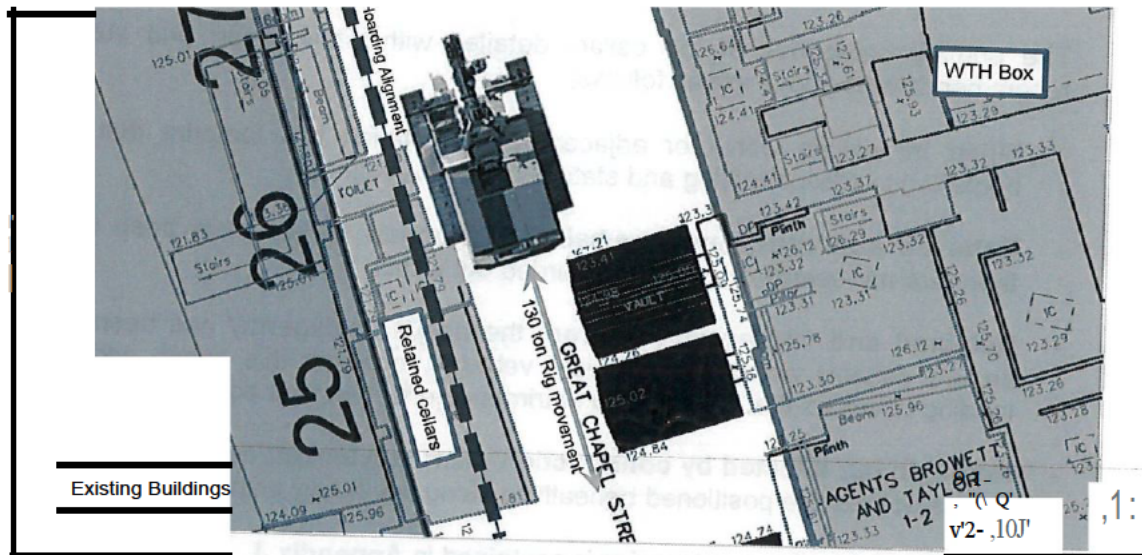


Figure 5. Example of proximity of cellars to large plant movements on Great Chapel Street



5 Conclusions and Recommendations

The design proposals contained within the C134 drawings ignores the presence of cellar spaces. C422 has not modified the basic C134 kerblines which permit traffic to overrun cellars, where previously it was not permitted. In order to progress the design, the following recommendations are made;

1. The backfill to basement and cellars is not suitable for the construction of the urban realm works above. We recommend that the Consultant is instructed to consider "full depth" pavement design in these areas
2. To determine what potential deterioration in cellar strength may have occurred, this report is to be issued to a structural engineer with experience of older structures for review
3. Further to this, a review of the existing situation is undertaken by a construction methodology engineer who can comment upon the potential impacts that may have occurred during the construction period. In particular upon cellars that are retained but have had heavy plant and construction activities nearby
4. There may be obligations upon CRL to ensure all the cellars that are retained are in the same condition as the pre-construction stage. CRL will need to establish what rights of entry they have for surveys
5. Lastly, the presence of grout shafts and temporary walls is known about. Drawings provided on 9 August 2016 are not "as-builts", showing the post grouting situation. It is recommended that the Consultant be instructed to consider this in more detail and to include the full depth pavement options around all grout shafts. The Consultant considers that this should be a location wide instruction, to cover all other excavations, because it is apparent backfill has not been placed to an adequate standard

The primary categories of the cellars detailed within this report and subject to the recommendations above are as follows;

Filled in; cellars within or adjacent to the station box footprint that have been backfilled to allow enabling and station works to progress

Retained and carriageway remains unchanged; cellars have been retained and there are no records of any works carried out within

Retained and carriageway altered; the above carriageway has been modified to ensure suitable access for relevant vehicles, however this brings additional direct loading to cellars that may have a detrimental effect on their condition

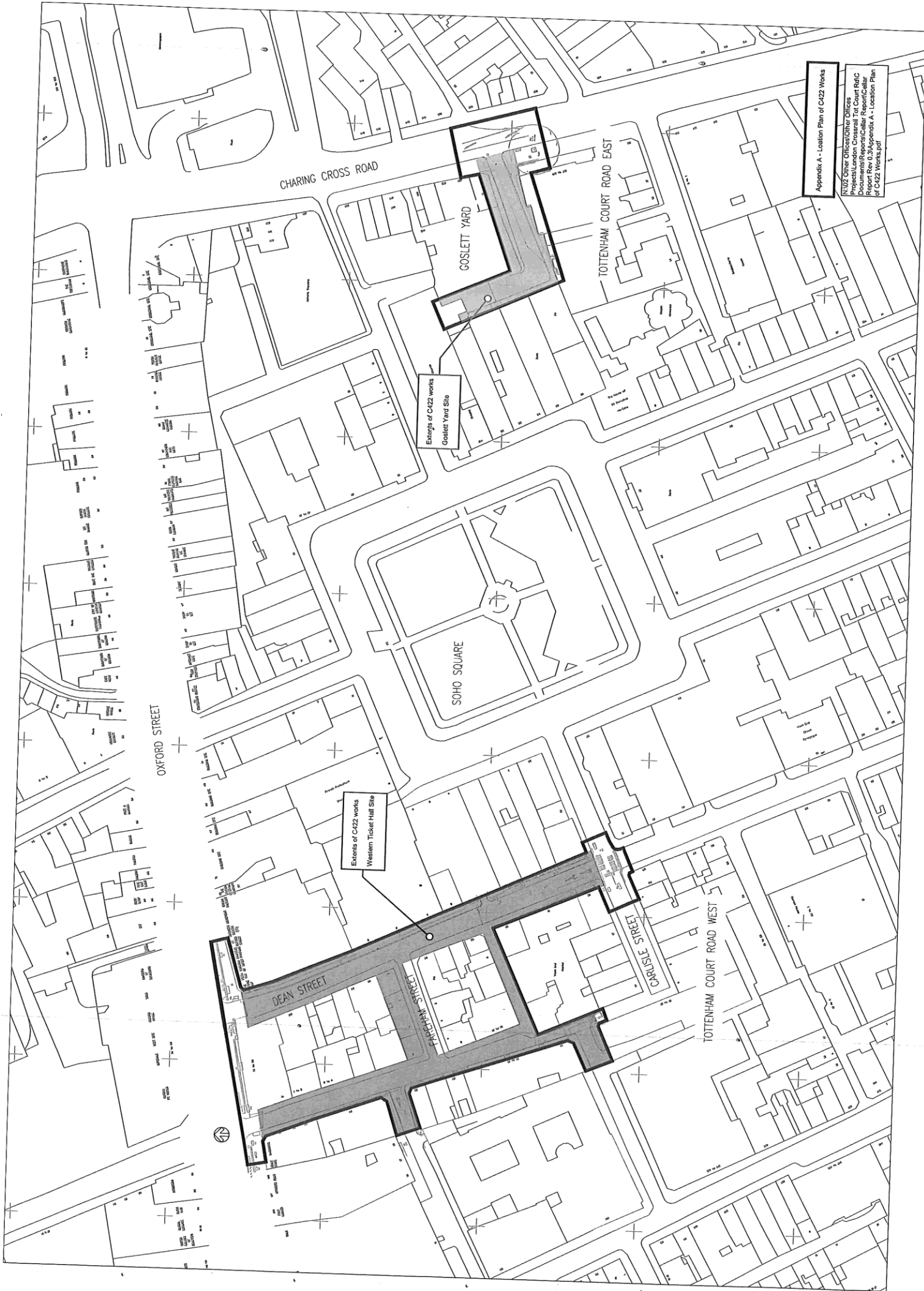
Retained but affected by construction plant movement; as aforementioned certain cellars spaces are positioned beneath or alongside heavy plant working zones

Plans showing the above categories is contained in **Appendix J**.

6 Appendices

- 6.1 Appendix A - Location Plan of C422 Works
- 6.2 Appendix B - Cellar Information Obtained
- 6.3 Appendix C - Existing Cellar Reference Drawings
- 6.4 Appendix D - Schedule of Existing Cellar Conditions
- 6.5 Appendix E - Grout Shaft Drawings received 9 August 2016
- 6.6 Appendix F - Schedules of Cellars Remaining
- 6.7 Appendix G - WEE Response to Hollen Street Cellar Issue
- 6.8 Appendix H - Pumping Appliance Swept Path amendment at Hollen Street Junction
- 6.9 Appendix J - Categorised Cellars Site Wide

6.1 Appendix A - Location Plan of C422 Works



CHARING CROSS ROAD

GOSLETT YARD

TOTTENHAM COURT ROAD EAST

Extent of C422 works
Goslett Yard Site

SOHO SQUARE

OXFORD STREET

Extent of C422 works
Western Ticket Hall Site

DEAN STREET

SPENCER STREET

CARRISLE STREET

TOTTENHAM COURT ROAD WEST

Appendix A - Location Plan of C422 Works
K107 Office of the Chief Architect
Project London Call Centre
Documents/Reports/Call Centre/Call Centre
Report Rev 0.3/Appendix A - Location Plan
of C422 Works.pdf



6.2 Appendix B - Cellar Information Obtained

6.2.1 Schedule of all documents/information obtained

6.2.2 Early Warning Notice C422-EWN-00532

including two photographs of an example cellar backfilled with unsuitable material

6.2.3 Cellar Survey - Northwest

6.2.4 Cellar Survey - Southwest

6.2.5 Cellar Survey - East

6.2.6 C134-OVE-D-DDA-N105-1-00201 C01 -Western Entrance Existing
· Topographical and Basement Surveys ·

Appendix B - 6.2.1 Schedule of all documents / information obtained

ID	File Name	Document Title	Document Type	Any References to Basements?	3D Survey Included?
1	C208-M CG-01-STP-N 105-50002	C208 Tottenham Court Road - Western Ticket Hall Health and Safety File	Health and Safety File	Yes	No
2	C134-LU-O1-LRC-N105_WS09 3-50001	Health and Safety File for the Goslett Yard Box and Escalator Decline	Health and Safety File	No	No
3	C300-BFK-Z 7-STP-CRT00_ST005- 50041 - Version 2	C300/ 410 Western Tunnels & Caverns Project Health and Safety File	Health and Safety File	No	No
4	C300-BFK-Z 7-STP-CRT00_ST 00 5-50041 - Version 3	C300/410 Western Tunnels & caverns Project Health and Safety File	Health and Safety File	No	No
5	C421 -BBM -C- RGN-N 105 _WS089- 50115	C42_1 Tottenham Court Road - Western Ticket Hall Health & Safety File Grout Shaft 7	Health and Safety File	No	No
6	C421-BBM-27-RGN-N 10,5_WS089-50005	C4 21 Tottenham Court Road - Western Ticket Hall Health & Safety File	Health and Safety File	No	No
7	C422-1000-COM -SCC- 1073 CC HSF file req LOR to CRL 151204	CC40 5 - Health and Safety Information	Health and Safety File	No	No
8	C4 22-EWN-00532	Employers Communication - Dean Street Urban Realm Voids and Subsidence	Early Warning Notice	Yes	No
9	Cellar Survey - Northwest	Cellar Survey - Northwest	Cellar Survey	Yes	Yes
10	Cellar Survey - South west	Cellar Survey - South west	Cellar Survey	Yes	Yes
11	Cellar Survey - East	Cellar Survey - East	Cellar Survey	Yes	Yes
12	C134-OVE- D-DDA-N105..._I- 0 0 201 C0I	Existing Topographical and Basement Surveys	Topo Survey	Yes	No

Note: The Health and Safety Files are not contained in Appendix B, only listed above

Note: Items 8 to 12 are contained in Appendix B following this table

N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\C#ar Report\Cellar Report Rev 0.3\Appendix B - Cellar Information Obtained.pdf

Date :09/03/2016

422 TOTTENHAM COURT ROAD STATION

AA:110 V1.ER*,S CO . WTH WI CA Tin&

PACKAGE : 000850 Design and Design Assurance
 CONTRACTOR Order No. : S-C3259/000002
 COMMUNICATION NUMBER : G0483 Source Document - Type: EWN No.: 00532
 Ref.:
 CONTRACTOR : WSP UK Ltd

SUBJECT: Dean Street Urban Realm - Voids and Subsidence

The Employer has raised Early Warning Notice No. 532 to the Project Manager regarding voids and subsidence which could potentially effect the Urban Realm works on Dean Street. The Employer issues the following text for the Consultant's information and will issue a further communication once a response has been received from the Project Manager.

Early Warning Notice C422-EWN-00632

"During the removal of reinforced concrete guidewall on Dean street between gridlines 4 and 5 the Contractor observed unexpected subsidence and uncovered unexpected brick arch structures. The structures have not been back filled with concrete. Photographs are attached to this EWN.

The Contractor is concerned that these voids could affect the urban realm works and the quality/durability of the architectural paving adjacent to the ground floor ticket hall entrance in particular.

The Contractor notes in C208 McGee health and safety file Appendix J the Project Manager Issued PMI/C208/105 to McGee's which de-scoped the removal of buried structures outside the main WTH box.

The Contractor also notes that the this Health and Safety file (C208-MCG-0 1-STP-N105-50002) has AOT been Instructed and does not form part of the Volume 2C document list, whereas the C300 and C421 have been instructed.

The Contractor will arrange an EWN meeting in due course. "

Appendix B - 6.2.2
Early Warning Notice C422-EWN-00532

For Laing O'Rourke Construction Ltd
Project Leader, Commercial Manager

Date

Originated by

9:3:16

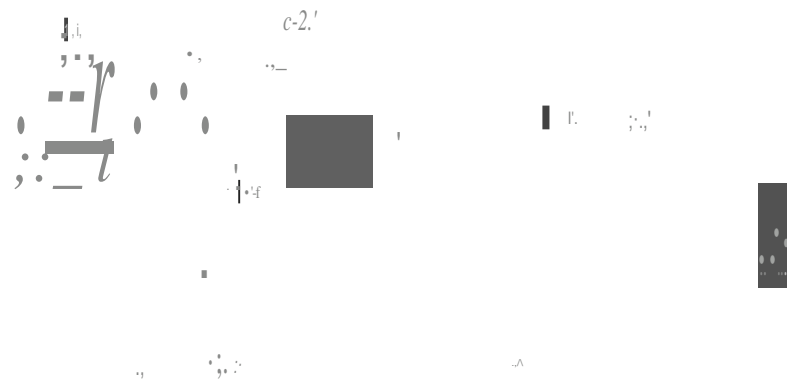
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Report Rev 0.3\Appendix B - Cellar Information Obtained.pdf



A Appendix B - 6.2.2
E Early Warning Notice C422-EWN-00532
P Photograph 1 - Unsuitable backfill within redundant cellar

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R Report Rev 0.3\Appendix B - Cellar Information Obtained.pdf



Appendix B - 6.2.2
Early Warning Notice C422-EWN-00532
Photograph 2 - Unsuitable backfill within redundant cellar

on Obtained.pdf

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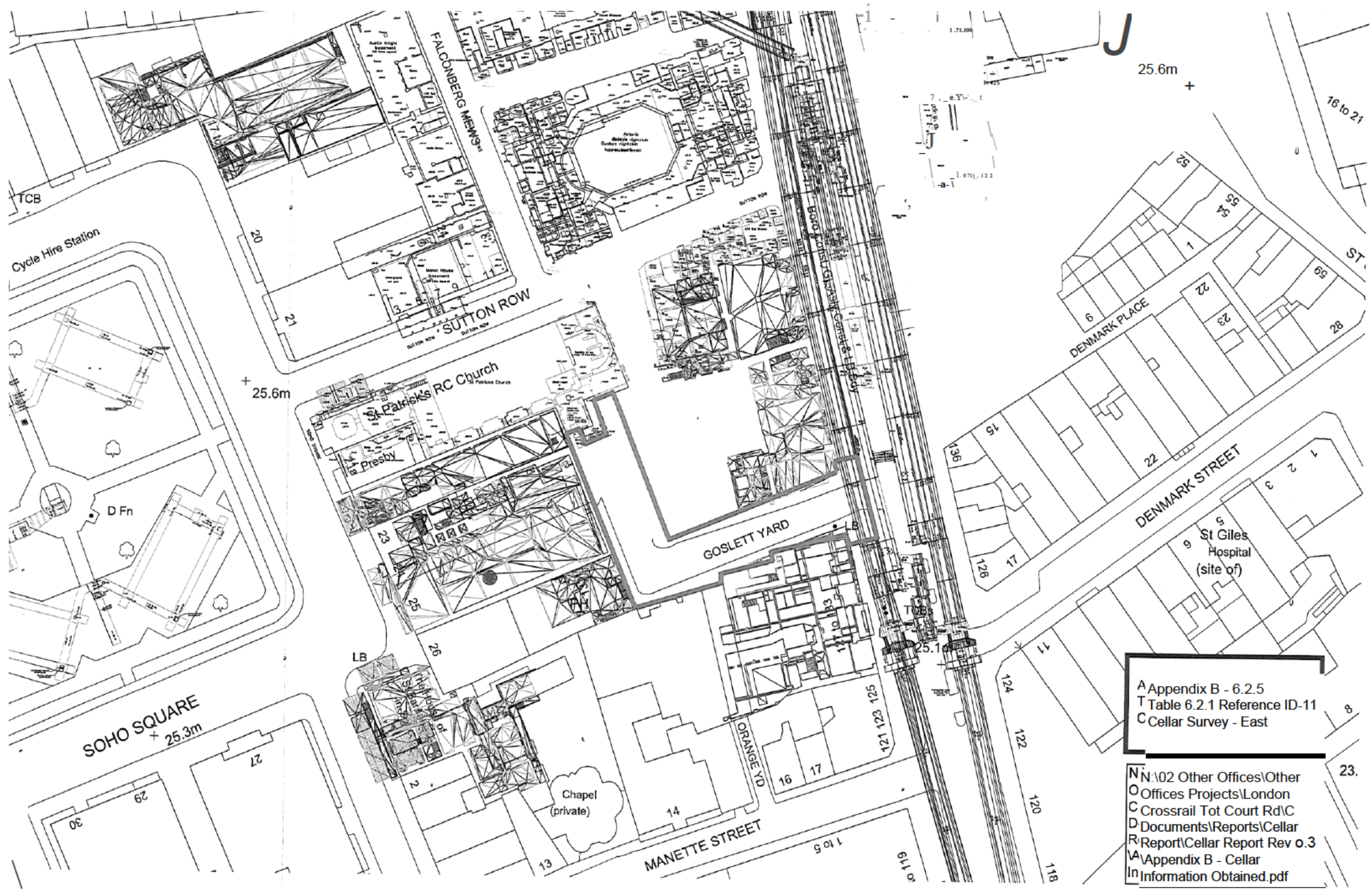
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A Appendix B - 6.2.5
 T Table 6.2.1 Reference ID-11
 C Cellar Survey - East

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 O Offices Projects\London
 C Crossrail Tot Court Rd\C
 D Documents\Reports\Cellar
 R Report\Cellar Report Rev o.3
 A Appendix B - Cellar
 In Information Obtained.pdf

6.3 Appendix C - Existing Cellar Reference Drawings

6.3.1 WSP-C422-SK-001 - Cellar Reference West

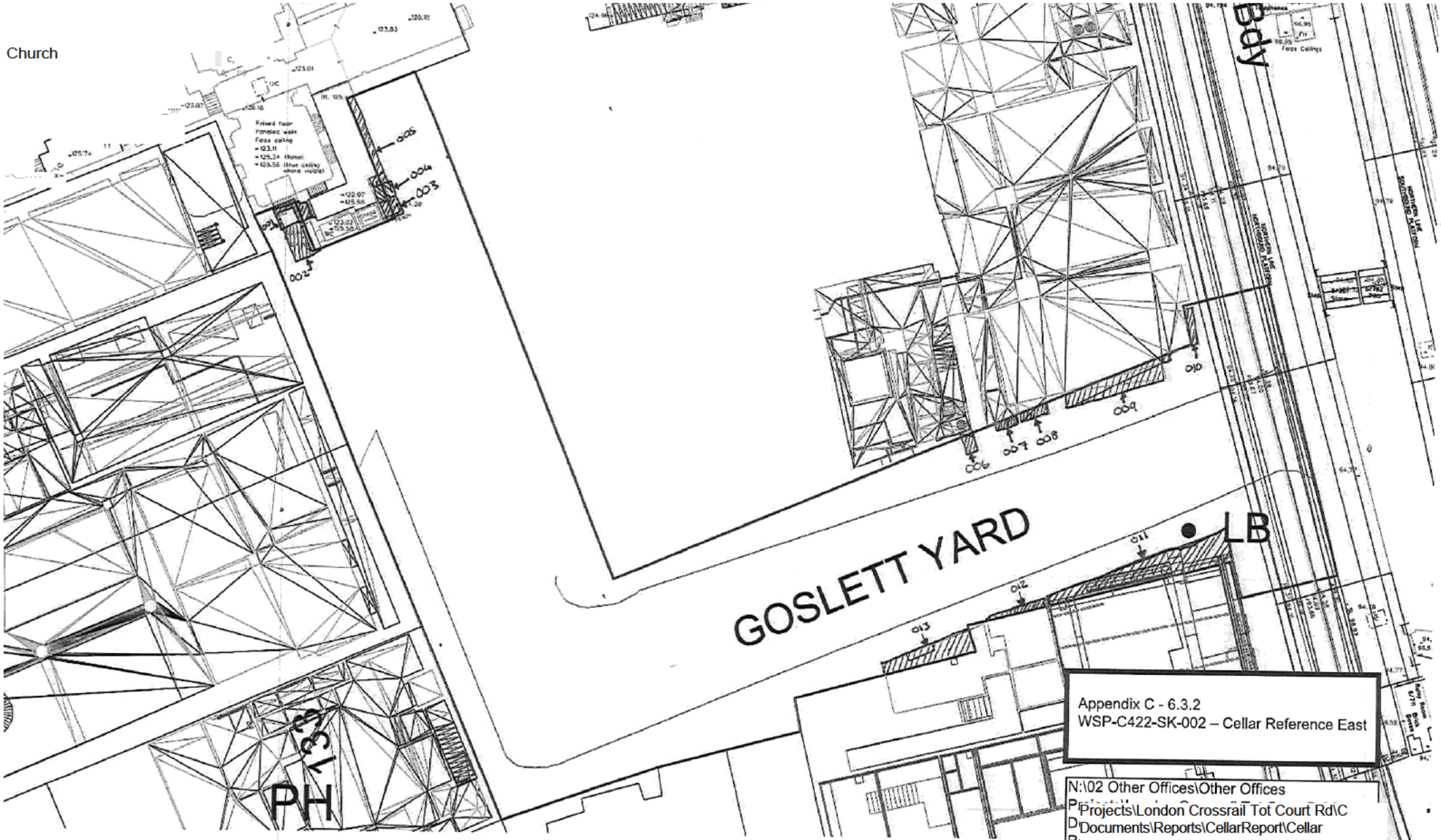
6.3.2 WSP-C422-SK-002 - Cellar Reference East

Appendix C-6.3.1
 VSP-C-22-5K-001 - Color Reference West
 Appendix C-6.3.1
 VSP-C-22-5K-001 - Color Reference West
 1422 Other Office/Other Offices
 Project/London Crossrail Test Court Plot
 Document/Other/Color Reference Report
 Rev 0.3/Appendix C - Existing Color Reference
 Drawings.pdf



13

Church



Appendix C - 6.3.2
 WSP-C422-SK-002 – Cellar Reference East

N:\02 Other Offices\Other Offices
 P\Projects\London Crossrail Tot Court Rd\C
 D\Documents\Reports\CellarReport\Cellar
 R\Report Rev 0.3\Appendix C - Existing Cellar
 Reference Drawings.pdf



6.4 Appendix D - Schedule of Existing Cellar Conditions

Side of road	Great Chapel Street	Filled in Y/N?	Information
West	GCS 001	N	
West	GCS 002	N	
West	GCS 003	N	
West	GCS 004	N	
West	GCS 005	N	
West	GCS 006	N	
West	GCS 007	N	
West	GCS 008	N	
West	GCS 009	N	
West	GCS 010	N	
West	GCS_011	N	
West	GCS 012	N	
West	GCS_013	N	
West	GCS 014	N	
West	GCS_015	N	
West	GCS 016	N	
West	GCS_017	N	
West	GCS 018	N	
West	GCS_019	N	
West	GCS 020	N	
West	GCS 021	N	
West	GCS 022	N	
West	GCS 023	N	
West	GCS 024	N	
West	GCS 025	N	
East	GCS_I00	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_I01	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 102	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_103	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 104	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_105	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_106	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 107	Y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.

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East	GCS_108	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_109	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_IIO	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 111	y	According to file C208-M CG-01 -STP-N105-50002 , this cellar has been removed and backfilled to pile mat formation level.
East	GCS_112	y	According to file C208-M CG-01 -STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.

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Cellar survey

Side of Road	Dean Street	Filled in Y/N?	Information
West	OS_001	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 002	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS_003	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS_004	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 005	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 006	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 007	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS_008	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 009	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 010	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS_011	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 012	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS 013	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	OS_014	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	OS 100	N	
East	OS 101	N	
East	OS 102	N	
East	OS 103	N	
East	OS 104	N	
East	OS 105	N	
East	OS 106	N	

**N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C
Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix D - Schedule of
Existing Cellar Conditions.pdf**

Cellar survey ""

East	OS_107	N	
East	DS 108	N	
East	DS_109	N	
East	OS 110	N	
East	OS 111	N	
East	OS 112	N	
East	OS 113	N	
East	OS 114	N	
East	OS 115	N	
East	DS 116	N	
East	DS 117	N	
East	OS 118	N	
East	DS 119	N	
East	DS 120	N	
East	OS 121	N	

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Cellar survey

Side of road	Diadem Court	Filled in Y/N?	Information
North	DC 001	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC_002	y	According to file C20 8-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC 003	y	According to file C20 8-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC 004	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC_005	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC_006	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC 007	y	According to file C208 -MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	DC 101	N	
South	DC 102	N	
South	DC 103	N	

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Cellar s;irvey

Side of road	Fareham Street	Filled in Y/N?	Information
North	FS 001	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 002	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 003	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 004	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	. FS 005	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS_006	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 007	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 008	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 101	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 102	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 103	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 104	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS_105	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS_106	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS_107	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.

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Goslett Yard	Filled in Y/N?	Information
GY 001	N	
GY 002	N	
GY_003	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
GY 004	y	According to file C208-MCG-OI-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
GY_00S	y	According to file C208-MCG-01-STP-N105-50002, this cella r has been removed and backfilled to pile mat formation level.
GY_006	y	According to file C208-M CG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
GY_007	y	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
GY 008	y	According to file C208 -M CG-01-STP- N105-5000 2, this cellar has been removed and backfilled to pile mat formation level.
GY 009	y	According to file C208-M CG-01-STP-N105-5000 2, this cellar has been removed and backfilled to pile mat formation level.
GY_010	y	According to file C208 -MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
GY 011	N	
GY 012	N	
GY 013	N	

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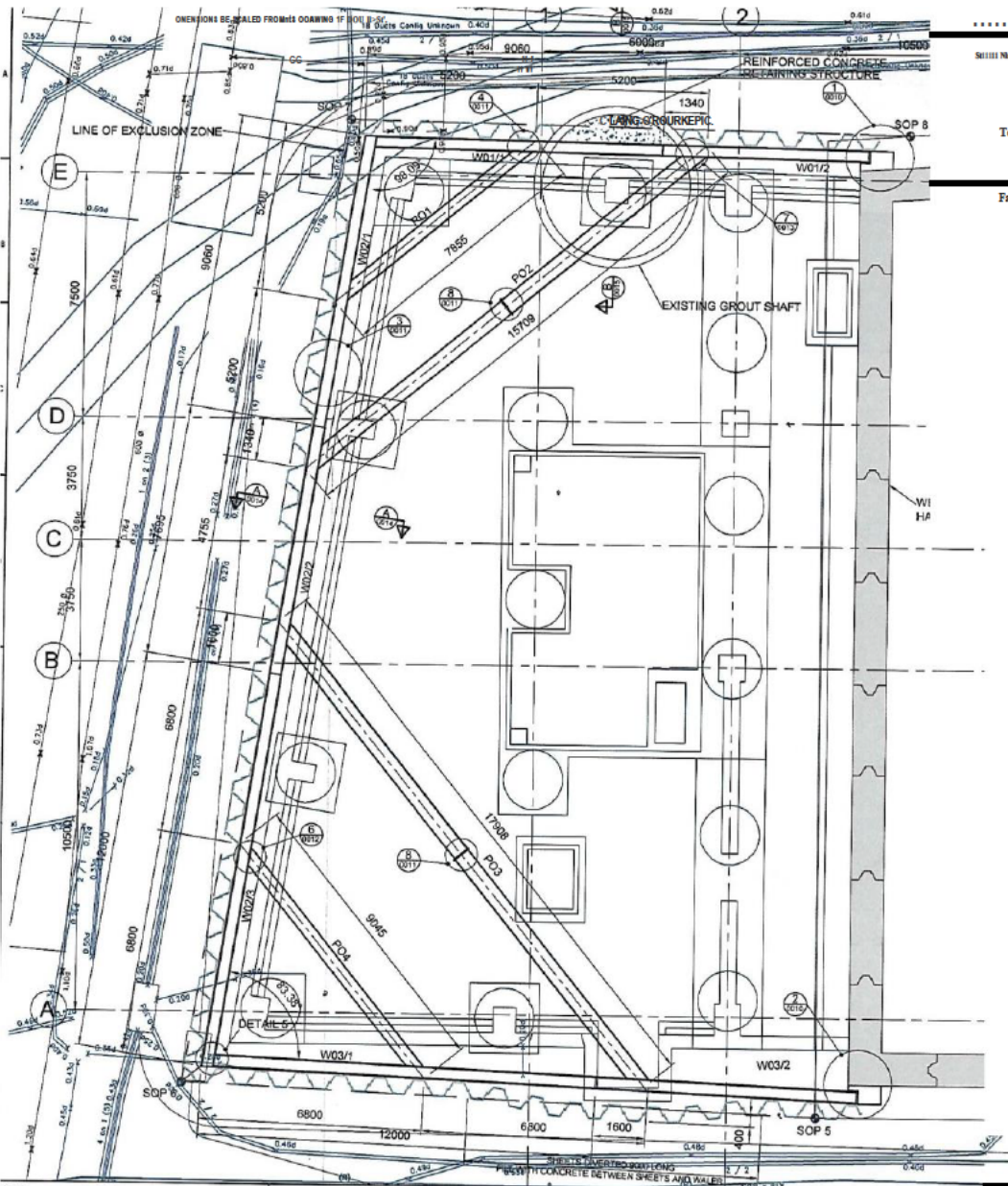


6.5 Appendix E - Grout Shaft Drawings received 9 August 2016

6.5.1 Oxford Street OSD Sheet Piling General Arrangement

6.5.2 Oxford Street OSD Sheet Piling RC Details

6.5.3 Western Entrance Crane Layout Site Plan



 SHEET No II
 10/1
 Tel: (01322)296200
 Fax: (01322)317786

GENERAL ARRANGEMENT

Orning

C272-ENG-TW-000

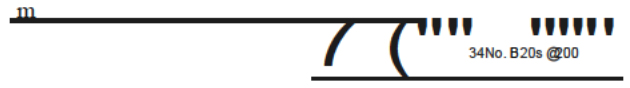
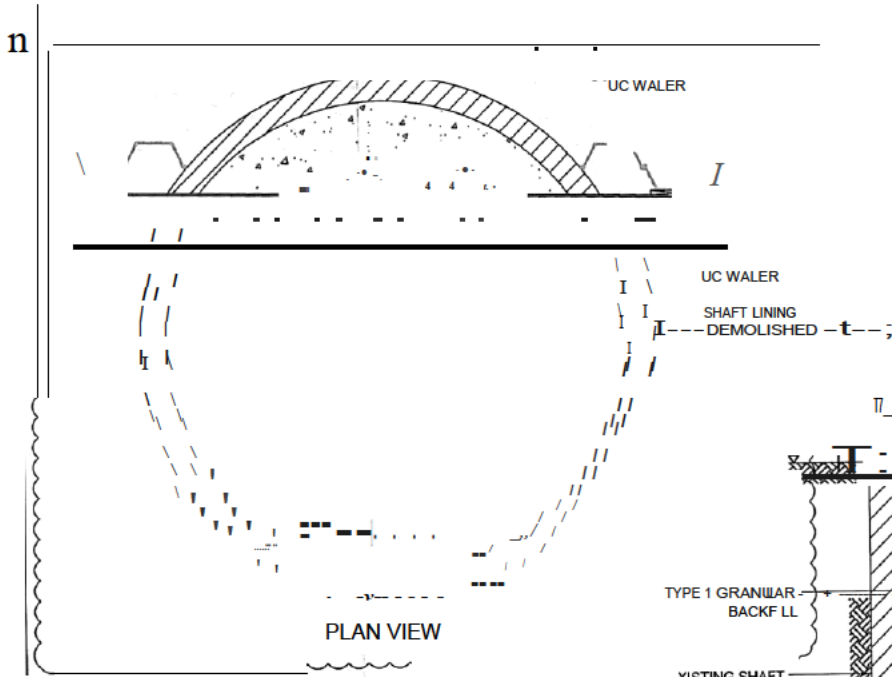
NOTES:
 1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER

NO.	REVISION	DATE

This Drawing is Based On The Following Information:
 (Changes For Subsequent Revisions Noted in Revision Column Above)



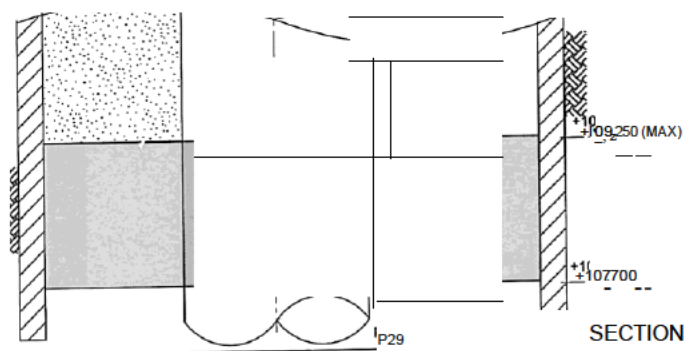
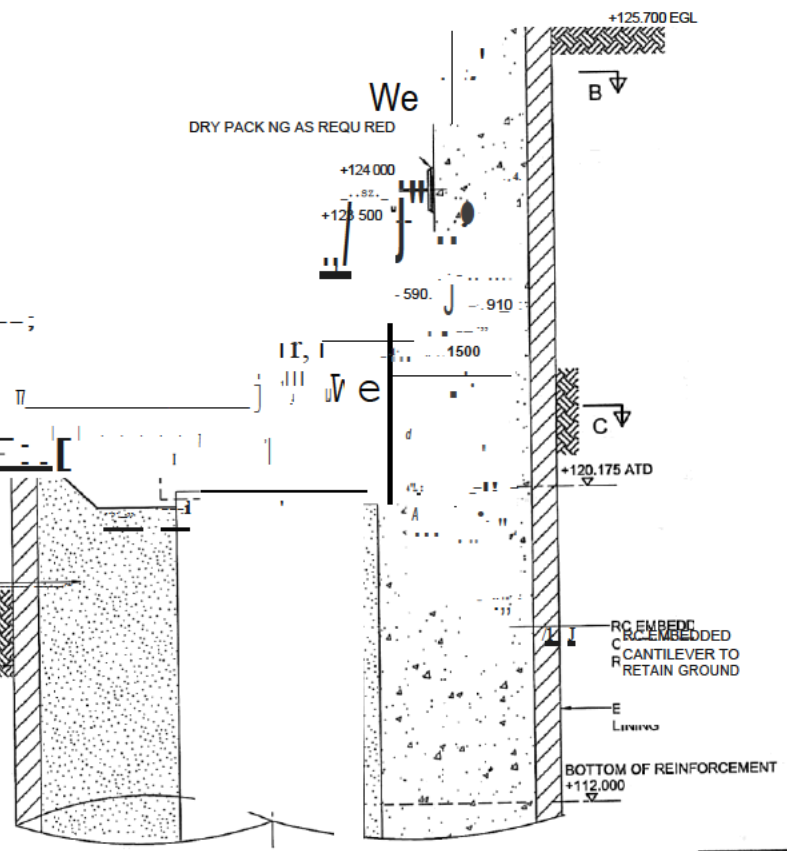
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SECTION B-B
REBAR DETAILS



SECTION C-C
REBAR DETAILS



SECTION A-A

NOTES:
1) REFER TO DRG. 0009 FOR NOTES

Appendix E - 6.5.2
Grout Shaft Drawings received 9 August 2016

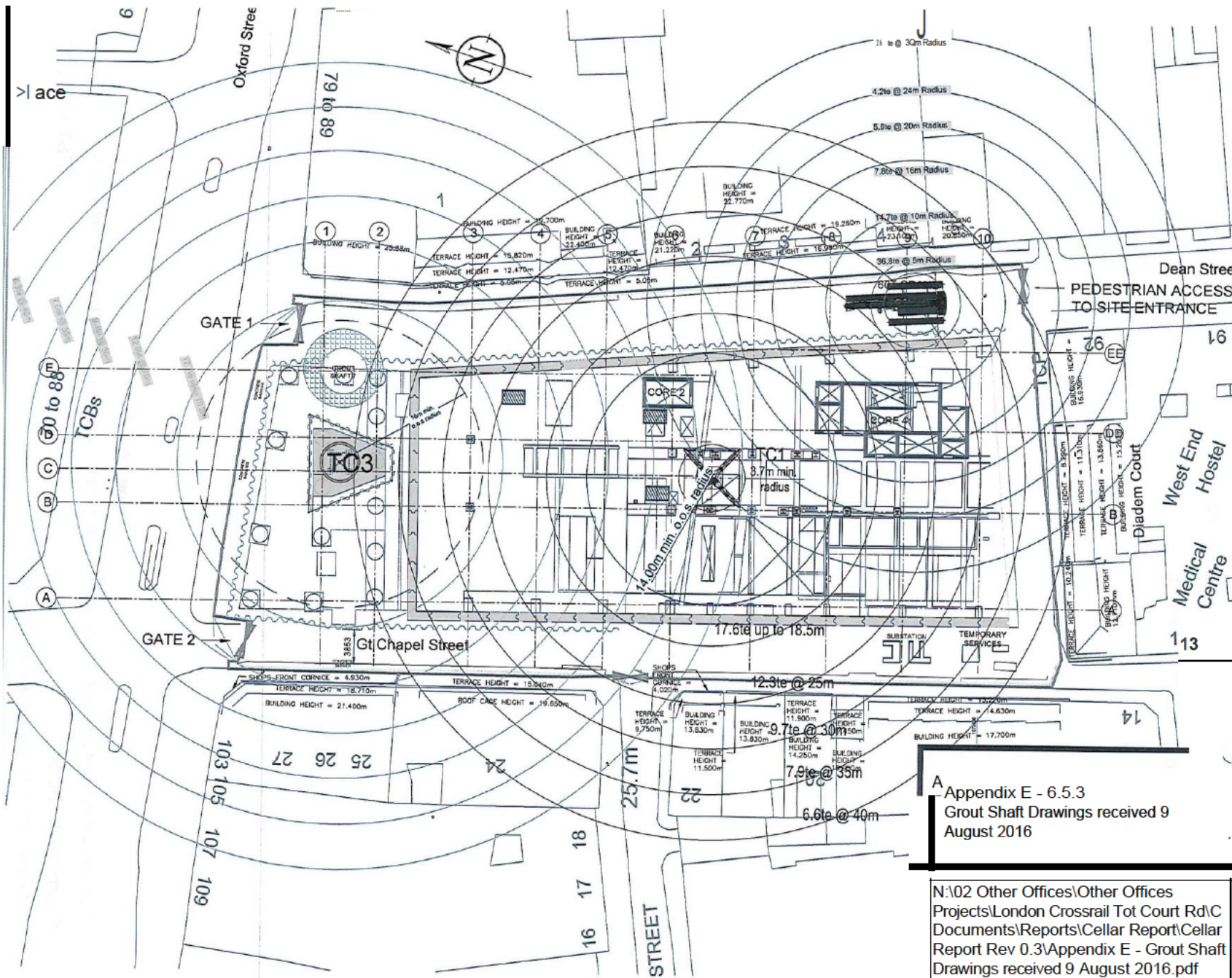
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Documents\Reports\Cellar Report\Cellar
Report Rev 0.3\Appendix E - Grout Shaft
Drawings received 9 August 2016.pdf

<p>1. MANNING OVERSEAS LTD RELEVANT: LONDON CROSSRAIL 21 L KNS1011E22mouacRECCDA1E1CaEHT011'VUJ</p>	<p>1. NO OTHER RELEVANT: LONDON CROSSRAIL 21 L KNS1011E22mouacRECCDA1E1CaEHT011'VUJ</p>	<p>THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF THE GROUT SHAFTS TO THE REQUIREMENTS OF THE CONTRACT DOCUMENTS AND THE LONDON CROSSRAIL DESIGN MANUAL FOR CONSTRUCTION (DMFC).</p>
---	---	--

LANGFORD & CO
BRIDGE PLACE
ADMIRALS PARK
DARTFORD
KEN LOA 285N
TEL (01322) 296200
FAX (01322) 311786

CROSSRAIL
TOTTENHAM COURT ROAD STATION
OXFORD STREET SHEET PILING
RC DETAILS

<p>1:10</p> <p>U&R BOC: F1 UR A/C m/ID</p> <p>C322ENG-nl-004</p>	<p>1/1</p> <p>P2</p>
--	----------------------



	HAKI STAIRS
	WALKWAY
	STATIC CONCRETE PUMP
	MORTAR SILO

- 1:1000;
- Under no circumstances should dimensions be scaled from this drawing - if in doubt ask.
 - This drawing is to be read in conjunction with all other relevant drawings, any discrepancies to be brought to the attention of the relevant engineer.

PCB Item drawing issue: 16/04/15
 Rev: 001/Will
 By: (111)
 Authorised

THIS DRAWING ORIGINATED IN THE FOLLOWING OFFICE:
 Laing O'Rourke, Tottenham Court Road Station Project,
 Upper Ground Floor, 10-11 Stephen Mews, London, W1T 1AQ
 Telephone: (01777) 332951



Project:
**C422
 TOTENHAM COURT ROAD**

**WESTERN ENTRANCE
 CRANE LAYOUT
 SITE PLAN**

ORIENTATION: 500 507 of 1
 J fu:-

A Appendix E - 6.5.3
 Grout Shaft Drawings received 9 August 2016

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 Drawings received 9 August 2016.pdf



6.6 Appendix F - Schedules of Cellars Remaining

6.6.1 Schedule of Cellars Left untouched

6.6.2 Schedule of Cellars Filled In

Side of Road	Reference	Information
Great Chapel Street		
West	GCS 001	No information to conclude if cellar is filled in.
West	GCS 002	No information to conclude if cellar is filled in.
West	GCS 003	No information to conclude if cellar is filled in.
West	GCS 004	No information to conclude if cellar is filled in.
West	GCS 005	No information to conclude if cellar is filled in.
West	GCS 006	No information to conclude if cellar is filled in.
West	GCS 007	No information to conclude if cellar is filled in.
West	GCS 008	No information to conclude if cellar is filled in.
West	GCS 009	No information to conclude if cellar is filled in.
West	GCS 010	No information to conclude if cellar is filled in.
West	GCS 011	No information to conclude if cellar is filled in.
West	GCS 012	No information to conclude if cellar is filled in.
West	GCS 013	No information to conclude if cellar is filled in.
West	GCS 014	No information to conclude if cellar is filled in.
West	GCS 015	No information to conclude if cellar is filled in.
West	GCS 016	No information to conclude if cellar is filled in.
West	GCS 017	No information to conclude if cellar is filled in.
West	GCS 018	No information to conclude if cellar is filled in.
West	GCS 019	No information to conclude if cellar is filled in.
West	GCS 020	No information to conclude if cellar is filled in.
West	GCS 021	No information to conclude if cellar is filled in.
West	GCS 022	No information to conclude if cellar is filled in.
West	GCS 023	No information to conclude if cellar is filled in.
West	GCS 024	No information to conclude if cellar is filled in.
West	GCS 025	No information to conclude if cellar is filled in.
Dean Street		
East	DS 100	No information to conclude if cellar is filled in.
East	DS 101	No information to conclude if cellar is filled in.
East	DS 102	No information to conclude if cellar is filled in.
East	DS_103	No information to conclude if cellar is filled in.
East	DS 104	No information to conclude if cellar is filled in.
East	DS 105	No information to conclude if cellar is filled in.
East	DS 106	No information to conclude if cellar is filled in.
East	DS -107	No information to conclude if cellar is filled in.
East	DS 108	No information to conclude if cellar is filled in.
East	DS 109	No information to conclude if cellar is filled in.
East	DS 110	No information to conclude if cellar is filled in.
East	DS 111	No information to conclude if cellar is filled in.
East	DS 112	No information to conclude if cellar is filled in.
East	DS_113	No information to conclude if cellar is filled in.
East	DS 114	No information to conclude if cellar is filled in.
East	DS_115	No information to conclude if cellar is filled in.
East	DS 116	No information to conclude if cellar is filled in.
East	DS 117	No information to conclude if cellar is filled in.
East	DS 118	No information to conclude if cellar is filled in.
East	DS 119	No information to conclude if cellar is filled in.
East	DS 120	No information to conclude if cellar is filled in.
East	DS 121	No information to conclude if cellar is filled in.
N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix F - Schedules of Cellars Remaining.pdf		

Diadem Court		
South	DC 101	No information to conclude if cellar is filled in.
South	DC 102	No information to conclude if cellar is filled in.
South	DC 103	No information to conclude if cellar is filled in.
Goslett Yard		
	GY_001	No information to conclude if cellar is filled in.
	GY_002	No information to conclude if cellar is filled in.
	GY_011	No information to conclude if cellar is filled in.
	GY_012	No information to conclude if cellar is filled in.
	GY_013	No information to conclude if cellar is filled in.

N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix F - Schedules of Cellars Remaining.pdf

Side of Road	Reference	Information
Great Chapel Street		
East	GCS_IO0	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 101	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_102	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 103	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_104	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 105	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_106	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_107	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 108	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 109	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_110	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS 111	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
East	GCS_112	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
Dean Street		
West	D5_001	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_002	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix F - Schedules of Cellars Remaining.pdf		

West	DS_003	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_004	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_005	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_006	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_007	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_008	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_009	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_010	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_011	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_012	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	D5_013	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
West	DS_014	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
Diadem Court		
North	DC_001	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC_002	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC_003	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC_004	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.

N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix F - Schedules of Cellars Remaining.pdf

North	DC 005	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC 006	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	DC 007	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
Fareham Street		
North	FS 001	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS_002	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 003	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 004	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 005	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS_006	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS_007	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
North	FS 008	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 101	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS_102	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 103	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS_104	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS 105	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix F - Schedules of Cellars Remaining.pdf		

South	FS_106	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
South	FS_107	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
Goslett Yard		
	GY_003	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_004	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_005	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_006	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_007	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_008	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_009	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
	GY_010	According to file C208-MCG-01-STP-N105-50002, this cellar has been removed and backfilled to pile mat formation level.
N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\C Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix F - Schedules of Cellars Remaining.pdf		



6.7 Appendix G- WCC Response to Hollen Street Cellar Issue

1. that the kerbline build out in Hollen Street was put in to protect the cellar,

And if this is not the case, that

2. Hollen Street can carry all UK legal vehicles irrespective of weight.

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Projects\London Crossrail Tot Court Rd\C
Documents\Reports\Cellar Report\Cellar
Report Rev 0.3\Appendix G - WCC
Response to Hollen Street Cellar Issue.pdf



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OHC
Oxford House College

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 **AREA MAP**



FINDING YOU DURING CROSSRAIL

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 \London Crossrail Tot Court Rd\C
 DC Documents\Reports\Cellar Report\Cellar
 R Report Rev 0.3\Appendix G - WCC
 R Response to Hollen Street Cellar Issue.pdf

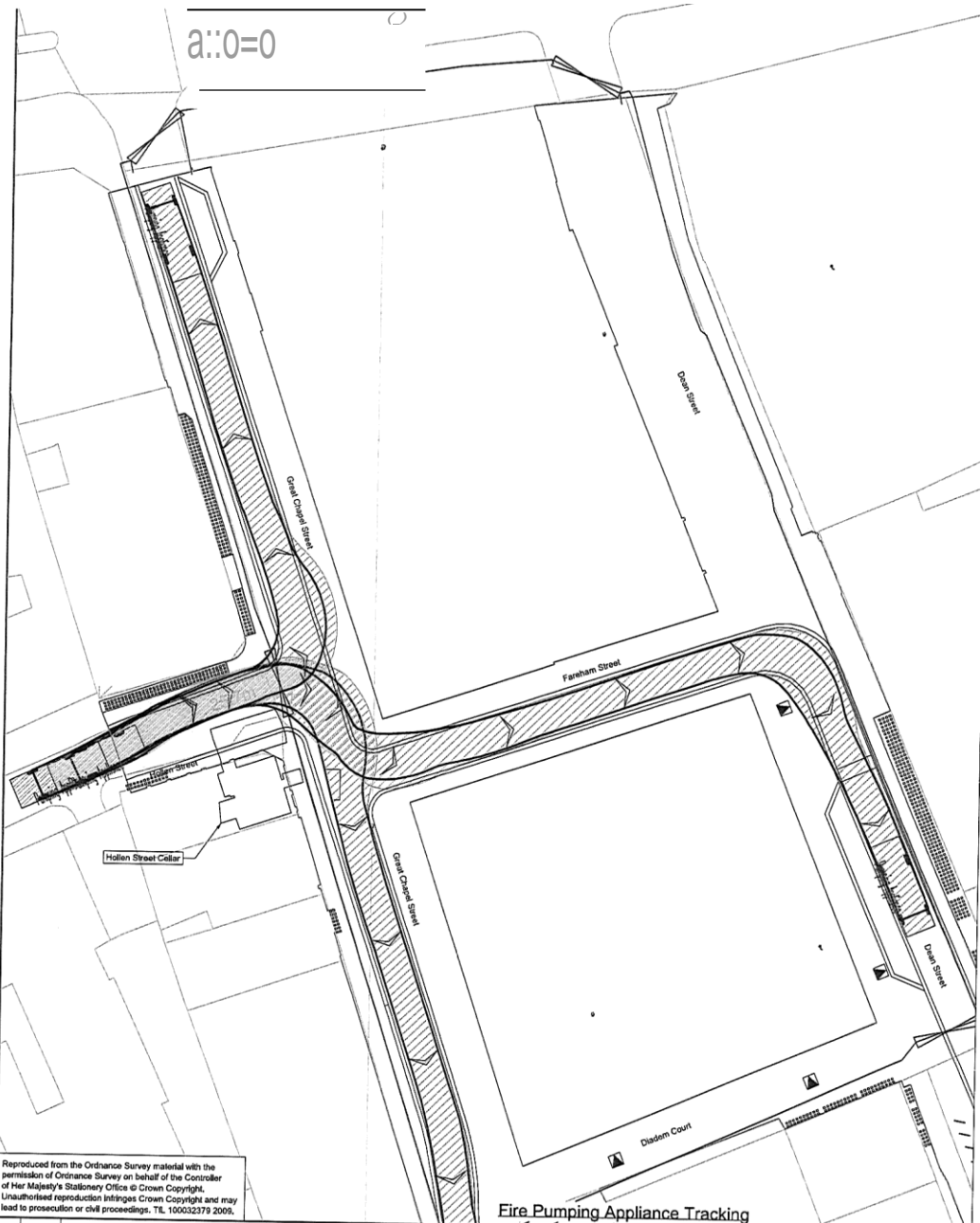
O'Rourke" include Laing O'Rourke Corporation Ltd and its affiliates ""V is it
www.laingorourke.com for more informat ion.

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\London Crossrail Tot Court Rd\C
Documents\Reports\Cellar Report\Cellar
Report Rev 0.3\Appendix G - WCC
Response to Hollen Street Cellar Issue.pdf



6.8 Appendix H - Swept Path amendment at Hollen Street Junction

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Safety, Health and Environmental Information
 There are no significant risks identified in this drawing beyond the expected knowledge of a competent contractor.
 These notes are based on the use of experienced and competent contractors carrying out the work using an approved safe method of working.

Key
 — Site boundary
 — Hoarding Line

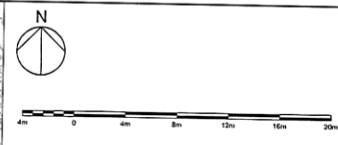
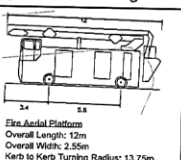
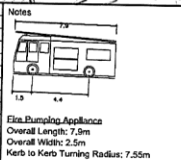
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Fire Pumping Appliance Tracking

Appendix H – Pumping Appliance Swept Path amendment at Hollen Street Junction

N:\02 Other Offices\Other Offices Projects\London Crossrail Tot Court Rd\Documents\Reports\Cellar Report\Cellar Report Rev 0.3\Appendix H – Pumping Appliance Swept Path amendment at Hollen Street Junction.pdf

Rev.	Date	Description	By	Chkd	App.	Auth.
P01	04/12/2015	First Issue				
P02	19/04/2016		MC	LR	JB	-
P03	25/04/2016		MC	LR	JB	-
Rev.	Date	Description	By	Chkd	App.	Auth.



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 London
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 www.crossrail.co.uk

Contract: Tottenham Court Road Station (Construction)
 Designer: Laing O'Rourke Construction Ltd
 Location: Tottenham Court Road Stn
 Title: Highway Works and Urban Realm Swept Paths for Large Fire Vehicles, Northwest
 Rev: ALLOWAN
 Drawn: LANKIN
 No: JURGINS
 Date: S4
 Scale: 1:200 @ A1
 Drawing and CAD file No: C422-LAO-D-DL-N105_1-10035
 Rev: P04
 Submittal: S4

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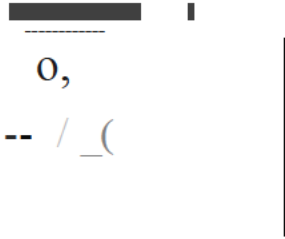


6.9 Appendix J - Categorised Cellars Site Wide

6.9.1 WSP-C422-SK-003 - Cellar

Reference West 6.9.2WSP-C422-SK-004 -
Cellar Reference East

- Cellars Back filled
- Cellars Retained but affected by Construction Traffic
- Cellars Retained but affected by Carriageway changes
- Cellars Unaffected



Appendix J - 6.9.1
 WSP-C422-SK-003 - Cellar Reference West

N 102 Other Offices/Other Offices
 Projects/Land on Crossrail 1 Tot Court Rd/C
 Documents/Reports/Collar Report/Cellar Report
 Rev 0.3/Appendix J - Categorized Cellars Site
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KEY

Cellars Backfilled

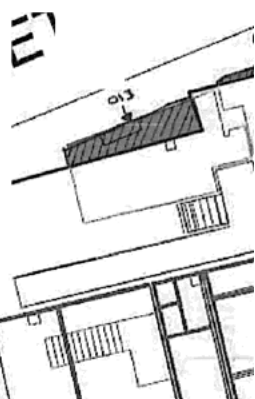
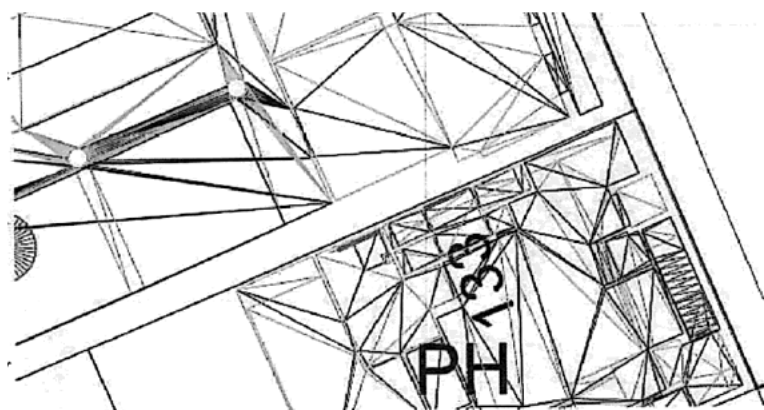
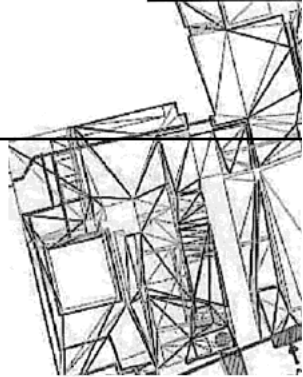
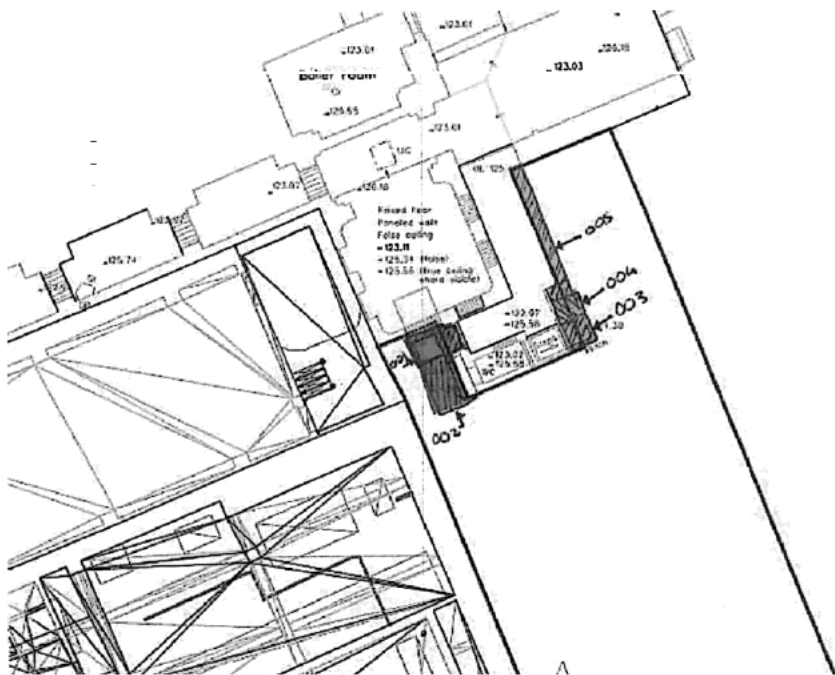
- Cellars Retained but affected by Construction Traffic

- Cellars Unaffected

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